WAKE ROBIN INN REDEVELOPMENT

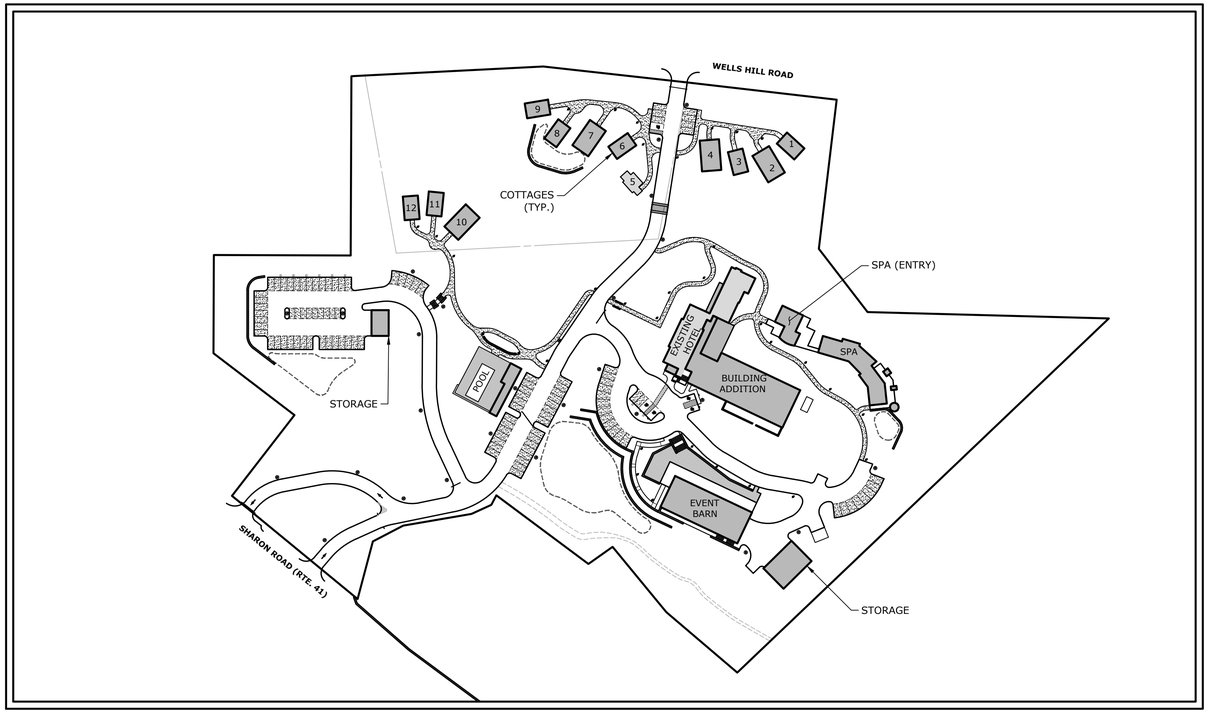
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SLR# 22100.00001

JULY 29, 2024
AUGUST 1, 2024
SEPTEMBER 6, 2024
NOVEMBER 6, 2024
NOVEMBER 26, 2024
DECEMBER 9, 2024

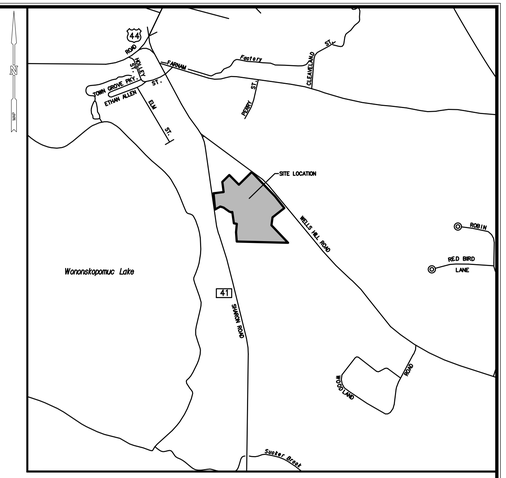
GENERAL NOTES

1. BOUNDARY AND TOPOGRAPHIC INFORMATION HAVE BEEN TAKEN FROM SURVEY ENTITLED "EXISTING CONDITIONS MAP", PREPARED BY ARTHUR H. HOWLAND & ASSOCIATES, P.C., PREPARED FOR ARADEV LLC, DATED AUGUST 1, 2024, SCALED 1"=60'.
2. NORTH ARROW AND BEARINGS ARE BASED UPON THE CONNECTICUT GRID SYSTEM (CTGS).
3. ELEVATIONS, CONTOUR AND BENCHMARKS ARE BASED UPON NAVD 1988.
4. INFORMATION REGARDING THE LOCATION OF EXISTING UTILITIES HAS BEEN BASED UPON AVAILABLE INFORMATION AND MAY BE INCOMPLETE, AND WHERE SHOWN SHOULD BE CONSIDERED APPROXIMATE. THE LOCATION OF ALL EXISTING UTILITIES SHOULD BE CONFIRMED PRIOR TO BEGINNING CONSTRUCTION. CALL "CALL BEFORE YOU DIG", 1-800-922-4455. ALL UTILITY LOCATIONS THAT DO NOT MATCH THE VERTICAL OR HORIZONTAL CONTROL SHOWN ON THE PLANS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION.
5. SLR INTERNATIONAL CORPORATION ACCEPTS NO RESPONSIBILITY FOR THE ACCURACY OF MAPS AND DATA WHICH HAVE BEEN SUPPLIED BY OTHERS.
6. INLAND WETLANDS AND WATERCOURSES ON SITE WERE DELINEATED IN THE FIELD ON APRIL 25 AND MAY 21, 2024 BY MATTHEW J. SANFORD, REGISTERED SOIL SCIENTIST FROM SLR CONSULTING.
7. A CTDEEP STORMWATER GENERAL PERMIT IS REQUIRED PRIOR TO INITIATION OF CONSTRUCTION.
8. ALL UTILITY SERVICES ARE TO BE UNDERGROUND. THE EXACT LOCATION AND SIZE OF ELECTRIC, TELEPHONE, CABLE TELEVISION, SANITARY SEWER AND PUBLIC WATER ARE TO BE DETERMINED BY THE RESPECTIVE UTILITY COMPANIES.
9. ALL STORM PIPING SHALL BE HIGH DENSITY POLYETHYLENE PIPE (HDPE) UNLESS OTHERWISE NOTED.
10. ALL PROPOSED CONTOURS AND SPOT ELEVATIONS INDICATE FINISHED GRADE.
11. ALL GRAVITY SANITARY SEWER PIPE SHALL BE SDR35 UNLESS OTHERWISE NOTED.
12. ALL FUEL, OIL, PAINT, OR OTHER HAZARDOUS MATERIALS USED ON SITE SHOULD BE STORED IN A SECONDARY CONTAINER AND REMOVED TO A LOCKED INDOOR AREA DURING NON-WORK HOURS.
13. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ANY UTILITIES INCLUDING IRRIGATION PIPES PRIOR TO THE START OF CONSTRUCTION.
14. ALL DIMENSIONS AND ELEVATIONS SHALL BE VERIFIED IN THE FIELD PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER.
15. SEDIMENT AND EROSION CONTROL MEASURES AS DEPICTED ON THESE PLANS AND DESCRIBED WITHIN THE SEDIMENT AND EROSION CONTROL NARRATIVE SHALL BE IMPLEMENTED AND MAINTAINED UNTIL PERMANENT COVER AND STABILIZATION IS ESTABLISHED. ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL CONFORM TO THE "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL - 2023, AS AMENDED, AND IN ALL CASES BEST MANAGEMENT PRACTICES SHALL PREVAIL.
16. ALL DISTURBED AREAS SHALL RECEIVE A MINIMUM OF 6" TOPSOIL AND BE SEEDED WITH SPECIFIED SEED MIX, AS SHOWN ON THE PLANS.
17. IN ALL CASES, TOPSOIL AND OTHER CONSTRUCTION MATERIALS SHALL BE DRAWN FROM THE ON-SITE STOCKPILES OF EXISTING MATERIAL. ONLY WHEN ON-SITE STOCKPILES HAVE BEEN USED SHALL MATERIAL BE IMPORTED TO THE SITE.
18. ALL CONSTRUCTION MATERIALS AND METHODS SHALL CONFORM TO THE TOWN OF SALISBURY REQUIREMENTS AND TO THE APPLICABLE SECTIONS OF THE STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, FORM 819 AND ADDENDUMS.
19. THE PLANS REQUIRE A CONTRACTOR'S WORKING KNOWLEDGE OF LOCAL, MUNICIPAL, WATER AUTHORITY, AND STATE CODES FOR UTILITY SYSTEMS. ANY CONFLICTS BETWEEN MATERIALS AND LOCATIONS SHOWN, AND LOCAL REQUIREMENTS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO THE EXECUTION OF WORK. THE ENGINEER WILL NOT BE HELD LIABLE FOR COSTS INCURRED TO IMPLEMENT OR CORRECT WORK WHICH DOES NOT CONFORM TO LOCAL CODE.
20. COMPLIANCE WITH THE PERMIT CONDITIONS IS THE RESPONSIBILITY OF BOTH THE CONTRACTOR AND PERMITEE.
21. THESE PLANS HAVE BEEN PREPARED FOR REGULATORY APPROVAL ONLY. THEY ARE NOT INTENDED FOR USE DURING CONSTRUCTION.
22. THE PROPERTY OWNER MUST MAINTAIN (REPAIR/REPLACE WHEN NECESSARY) THE EROSION CONTROLS UNTIL ALL DEVELOPMENT ACTIVITY IS COMPLETED AND ALL DISTURBED AREAS ARE PERMANENTLY STABILIZED.



PROJECT SITE VICINITY MAP:

0' 50' 100'
0 1/2" 1"
SCALE 1" = 100'



LOCATION MAP:

0' 500' 1000'
0 1/2" 1"
SCALE 1" = 1000'

LEGEND

EXISTING		PROPOSED
	STREET LINE	
	PROPERTY LINE	
	EASEMENT	
	SETBACK LINE	
NDDB	NDDB BOUNDARY	
70	MAJOR CONTOUR	70
68	MINOR CONTOUR	68
× 70.5	SPOT GRADE	+70.5
	WETLANDS	
	75' WETLANDS SETBACK	
	AQUIFER PROTECTION AREA BOUNDARY	
	TREE LINE	
	TREE/SHRUB	
	STONEWALL	
	SITE LIGHT	
HYD	HYDRANT	
	WATER METER	
WV	WATER VALVE	WV
GV	GAS VALVE	
	CATCH BASIN	
	MANHOLE/YARD DRAIN	
S	SANITARY SEWER SERVICE/MAIN	SAN
	STORM DRAIN W/CATCH BASIN	
W	WATER MAIN	W
E	ELECTRICAL CONDUIT	E
U	OVERHEAD WIRE	
	UTILITY POLE	
	TRAFFIC SIGN	
	MONUMENT	
	EDGE OF PAVEMENT W/CURB	

ZONING DATA TABLE

RURAL RESIDENCE 1 ZONE (RR-1)		
ADDRESS: 104 & 106 SHARON ROAD AND 53 WELLS HILL ROAD		
	REQUIRED/ALLOWED	PROVIDED
MIN. LOT AREA (104 & 106 SHARON ROAD)	80,000 SF	501,362 SF (11.5 ACRES)
MIN. LOT AREA (53 WELLS HILL ROAD)	80,000 SF	99,518 SF (2.3 ACRES)
MIN. LOT AREA (TOTAL)	80,000 SF	600,880 SF (13.8 ACRES)
MIN. BUILDABLE AREA	20,000 SF	> 20,000 SF
MIN. STREET FRONTAGE	25'	>25'
MIN. FRONT SETBACK	40'	45.4' (COTTAGE 9)
MIN. SIDE SETBACK	30'	36.7' (COTTAGE 1)
MIN. REAR SETBACK	30'	N/A
MIN. SQUARE EACH SIDE	150'	150'
MAX. BUILDING COVERAGE	10%	6.8%
MAX. IMPERVIOUS SURFACE COVERAGE	---	17.2%
MAX. BUILDING HEIGHT (INN)	52' (EXISTING)	<52' (PROP. ADDITION)
MAX. BUILDING HEIGHT (OTHER)	30' (FLAT ROOF)/35' (OTHER ROOF)	30'/35'
MIN. SEPARATION BETWEEN BUILDINGS	10'	10'

PARKING DATA

	COUNT
PERMANENT PARKING SPACES	111
ADA ACCESSIBLE PARKING SPACES	5
OVERFLOW GREAT LAWN SPACES	39
TOTAL PARKING SPACES	150 (111+39)

PER TABLE 703.11 TABLE OF PARKING REQUIREMENTS
1 SPACE PER ROOM; ADDITIONAL FOR OTHER FACILITIES BASED ON PARKING NEEDS ASSESSMENT

AQUIFER PROTECTION AREA DATA

	AREA (SF)
PROPERTY AREA WITHIN AQUIFER PROTECTION	248,640 SF (5.71 ACRES)
IMPERVIOUS AREA WITHIN AQUIFER PROTECTION	24,720 SF (0.57 ACRES)
PERCENTAGE OF IMPERVIOUS AREA	10.0%

811 Know what's below. Call before you dig. www.cbyd.com

PREPARED BY:

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

STATE OF CONNECTICUT · RICHARD D. BYRNE · No. 23730 · LICENSED PROFESSIONAL ENGINEER

PREPARED FOR:

ARADEV LLC
352 ATLANTIC AVENUE, UNIT 2
BROOKLYN, NY 11217

LIST OF DRAWINGS

NO.	NAME	TITLE
01	--	TITLE SHEET
02	EX	EXISTING CONDITIONS
03	RP	SITE PLAN - REMOVALS
04	LA	SITE PLAN - LAYOUT
05	LS	SITE PLAN - LANDSCAPING
06	GR	SITE PLAN - GRADING
07	UT	SITE PLAN - UTILITIES
08	PP-1	PHASING PLAN
09	PP-2	PHASING PLAN NOTES
10	SE-1	SEDIMENT & EROSION CONTROL PLAN
11	SE-2	SEDIMENT & EROSION CONTROL NOTES & DETAILS
12-20	SD-1 - SD-9	SITE DETAILS
21	STR-1	STRUCTURAL DETAILS
22	VM-1	VEHICULAR TURNING MOVEMENTS PLAN - SU-30
23	VM-2	VEHICULAR TURNING MOVEMENTS PLAN - SU-40
1 OF 1	SL-IC	SITE LIGHTING PHOTOMETRIC CALCULATION

EXISTING TREE LEGEND

- DEAD
- POOR
- FAIR
- GOOD

NOTES:

1. TREE INVENTORY WAS CONDUCTED ON SEPTEMBER 30 - OCTOBER 2, 2024 BY TIM ARMSTRONG OF BARTLETT TREE EXPERTS, CONSULTING ARBORIST, ASCA REGISTERED CONSULTING ARBORIST #790, ASCA TREE AND PLANT APPRAISAL QUALIFIED, ISA BOARD CERTIFIED MASTER ARBORIST #NE-7132B, MASSACHUSETTS CERTIFIED ARBORIST #2464, ISA TREE RISK ASSESSMENT QUALIFIED

0' 30' 60'
0 1/2" 1"

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DESCRIPTION	DATE	BY
P&Z SUBMISSION	8/1/2024	SM
PEER REVIEW COMMENTS	9/9/2024	DSR
TOWN COMMENTS	11/6/2024	SM
PUBLIC HEARING COMMENTS	12/9/2024	TDR

EXISTING CONDITIONS

WAKE ROBIN INN REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	TR
DESIGNED	DRAWN	CHECKED

1"=60'
SCALE

JULY 29, 2024
DATE

22100.00001
PROJECT NO.

02 OF 23
SHEET NO.

EX
SHEET NAME

REMOVALS LEGEND

- DEMO BUILDING
- REMOVE BITUMINOUS CONCRETE
- TEMPORARY DUMPSTER LOCATION
- REMOVE EXISTING TREE

EXISTING TREE LEGEND

- DEAD
- POOR
- FAIR
- GOOD

REMOVALS NOTES:

1. INFORMATION REGARDING THE LOCATION OF EXISTING UTILITIES HAS BEEN BASED UPON AVAILABLE INFORMATION AND MAY BE INCOMPLETE, AND WHERE SHOWN SHOULD BE CONSIDERED APPROXIMATE. THE LOCATION OF ALL EXISTING UTILITIES SHOULD BE CONFIRMED PRIOR TO BEGINNING CONSTRUCTION. CALL "CALL BEFORE YOU DIG", 1-800-922-4455. ALL UTILITY LOCATIONS THAT DO NOT MATCH THE VERTICAL OR HORIZONTAL CONTROL SHOWN ON THE PLANS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION.
2. INFORMATION SHOWN ON THIS DRAWING IS TO BE USED FOR REFERENCE ONLY. THE LOCATION, SIZE AND ELEVATIONS OF UTILITIES AND STRUCTURES AND THE NATURE OF THEIR CONTENTS SHALL BE CONFIRMED IN THE FIELD PRIOR TO DEMOLITION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO DEMOLITION.
3. THE INTENT OF THIS DRAWING IS TO IDENTIFY SPECIFIC DEMOLITIONS. HOWEVER, THE GRAPHIC LEGEND MAY NOT BE A COMPREHENSIVE LIST OF ALL SITE REMOVALS.
4. ABANDONED UTILITIES ARE TO BE REMOVED IN AREAS WHERE THEY CONFLICT WITH PROPOSED UTILITY DEPTHS AND LOCATIONS.
5. CONTRACTOR TO COORDINATE ALL PROPOSED UTILITY WORK WITH APPROPRIATE UTILITY COMPANIES. THIS WORK SHALL BE DONE AT THE CONTRACTORS EXPENSE UNLESS OTHERWISE PROVIDED FOR IN THE SPECIFICATIONS. THE CONTRACTOR SHALL REMOVE OR ABANDON THE EXISTING UTILITY SERVICES AS DIRECTED AND TO THE SATISFACTION OF EACH INDIVIDUAL UTILITY COMPANY.
6. ALL UNDERGROUND UTILITIES NOT SHOWN TO BE REMOVED SHALL HAVE SERVICE MAINTAINED AND SHALL BE PROTECTED DURING CONSTRUCTION.
7. CONTRACTOR IS RESPONSIBLE TO OBTAIN PERMITS REQUIRED AND COMPLY WITH ALL REGULATIONS IN THE DEMOLITION AND REMOVAL OF THE DESIGNATED STRUCTURES.
8. INSTALL ALL SEDIMENT AND EROSION CONTROLS PRIOR TO BEGINNING DEMOLITION WORK.
9. ALL TREES/VEGETATION IN CONFLICT WITH PROPOSED WORK SHALL BE REMOVED WHETHER NOTED ON THE PLANS OR NOT.

DISCONNECT AND DECOMMISSION EXISTING SANITARY SERVICE IN ACCORDANCE WITH TAHD AND WPCA REQUIREMENTS
NDDB BOUNDARY
DEMO BUILDING (TYP.)
TEMPORARY DUMPSTER PAD LOCATION
PRESERVE STEPS AND RETAINING WALL
WELLS HILL ROAD
50' WATERCOURSE SETBACK (TYP.)
REMOVE EXISTING AT GRADE SANITARY SEWER PIPE
75' WETLAND SETBACK (TYP.)
REMOVE FENCE AND GATE
REMOVE TRENCH DRAINS
INTERMITTENT WATERCOURSE
REMOVE PIPE SEE UTILITIES PLAN
APPROXIMATE LOCATION OF EXISTING POOL STRUCTURE REMOVE AND BACKFILL
WOOD BRIDGE TO BE REMOVED
WOOD BRIDGE TO BE REMOVED
REMOVE FENCE
WETLAND BOUNDARY (TYP.)
REMOVE BITUMINOUS CONCRETE DRIVEWAY (TYP.)
TEMPORARY DUMPSTER LOCATION
TEMPORARY DUMPSTER PAD LOCATION
REMOVE UTILITY POLE AND OVERHEAD WIRES (TYP.)
REMOVE DRAINAGE PIPES
REMOVE DRAINAGE PIPE AND YARD DRAIN
INTERMITTENT WATERCOURSE
SHARON ROAD (RTE. 41)
N/F Michael E. Rogers Vol. 134 Pg. 862 S.L.R.
N/F Charles & Sandra Kalison Vol. 173 Pg. 186 S.L.R.
N/F Moisha K. & Robert Blechman Vol. 263 Pg. 165 S.L.R.
N/F Todi Canie Vol. 241 Pg. 1101 S.L.R.
N/F FB Square Lk LLC Vol. 271 Pg. 352 S.L.R.
N/F Paul & Elaine Watson Vol. 262 Pg. 378 S.L.R.
N/F Jack Hawley II Vol. 247 Pg. 254 S.L.R.
PARCEL 'A' 2.277± ACRES MAP 2176 "EXCEPTED FROM PARCEL ONE
PARCEL ONE 11.250± ACRES
AT LEASE AREA 10,000 S.F. /0.230 AC. RECORDED IN VOL.262 PG.380 S.L.R.
L&P / EVERSOURCE EASEMENT AREA (SEE DEED) RECORDED IN VOL.71 PG.95 S.L.R. DELINEATED ON MAP 740 S.L.R.
25' WIDE RIGHT-OF-WAY IN FAVOR OF "HAWLEY" RECORDED IN BK.209 PG.95 S.L.R. DELINEATED ON MAP 740 S.L.R.
LIMITS OF RIGHT-OF-WAY "OVER EXISTING DRIVEWAY" RECORDED IN DEEDS VOL.40 PG.590, VOL.42 PG.375, VOL.57 PG.72,VOL.58 PG.17, VOL.74 PG.515, VOL.245 PG.915 & DELINEATED ON MAP 1436 S.L.R.
LIMITS OF RIGHT-OF-WAY "OVER EXISTING DRIVEWAY" RECORDED IN DEEDS VOL.57 PG.99, VOL184 PG.509, VOL.207 PG.215 & DELINEATED ON MAP 2176 S.L.R.

DESCRIPTION	DATE	BY
PEER REVIEW COMMENTS	8/6/2024	DSR
TOWN COMMENTS	11/6/2024	SM
PEER REVIEW COMMENTS	11/26/2024	SM

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SITE PLAN - REMOVALS

WAKE ROBIN INN REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

DESIGNED	DRAWN	CHECKED
SM	SM	TR

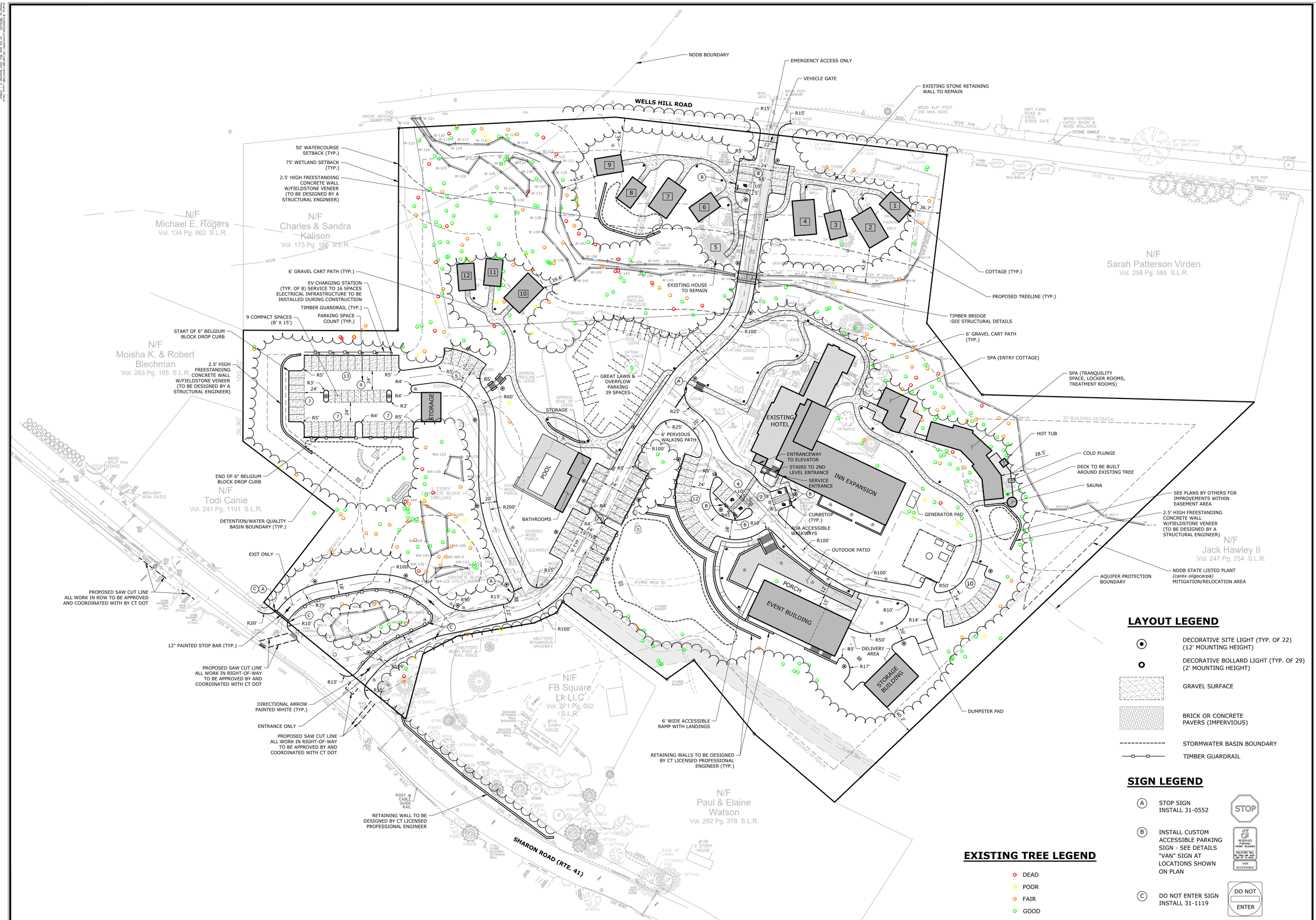
SCALE: 1"=50'

DATE: AUGUST 1, 2024

PROJECT NO.: 22100.00001

SHEET NO.: 03 OF 23

SHEET NAME: RP



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DESCRIPTION	DATE	BY
P&Z SUBMISSION	8/1/2024	SM
PEER REVIEW COMMENTS	9/9/2024	DSR
TOWN COMMENTS	11/6/2024	SM
PEER REVIEW COMMENTS	11/26/2024	SM
PUBLIC HEARING COMMENTS	12/9/2024	TDR

SITE PLAN - LAYOUT

WAKE ROBIN INN REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

DESIGNED	DRAWN	CHECKED
MA	SM	MA

SCALE: 1"=50'
DATE: JULY 29, 2024
PROJECT NO.: 22100.00001
SHEET NO.: 04 OF 23
SHEET NAME: LA

PROPOSED PLANT PALETTE

SHADE TREES	83	KEY	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS
		AR	Acer rubrum 'Autumn Flame'	Autumn Flame Red Maple	3"-3.5" CAL	B & B
		AS	Acer saccharum ' Green Mountain'	Green Mountain Sugar Maple	3"-3.5" CAL	B & B
		QA	Quercus alba	White Oak	3"-3.5" CAL	B & B
		LS	Liquidambar styraciflua	Sweet Gum	3"-3.5" CAL	B & B
		LT	Liriodendron tulipifera	Tulip Poplar	3"-3.5" CAL	B & B
		PO	Platanus occidentalis	American Sycamore	3"-3.5" CAL	B & B
		QA	Quercus bicolor	Swamp White Oak	3"-3.5" CAL	B & B
ORNAMENTAL TREES	131	KEY	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS
		AC	Amelanchier x g. 'Autumn Brilliance'	Autumn Brilliance Serviceberry	1"-2" CAL	B & B, Multistem
		AA	Amelanchier arborea	Downy Serviceberry	1"-2" CAL	B & B, Multistem
		BN	Betula nigra 'Heritage'	Heritage River Birch	10'-12' HT.	B & B, Multistem
		CF	Cornus florida	Flowering Dogwood	2"-2.5" CAL	B & B, Heavy
		CV	Crataegus viridis 'Winter King'	Hawthorn 'Winter King'	2"-2.5" CAL	B & B, Specimen
		CR	Cornus florida var. rubra	Pink Flowering Dogwood	2"-2.5" CAL	B & B, Multistem
		CC	Cercis canadensis	Eastern Redbud	1.5"-2" CAL	B & B, Specimen
EVERGREEN	94	KEY	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS
		CT	Chamaecyparis thyoides	Atlantic White Cedar	7'-8' HT.	B & B, Full & Dense
		JV	Juniperus virginiana	Eastern Red Cedar	7'-8' HT.	B & B, Full & Dense
		TO	Thuja Occidentalis	Arborvitae	10'-12' HT.	B & B, Full & Dense
		PG	Picea glauca	White Spruce	8'-10' HT.	B & B, Full & Dense
		PS	Tsuga canadensis	Eastern Hemlock	7'-8' HT.	B & B, Full & Dense
SHRUBS AND ORN. GRASS	1200	KEY	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS
		AI	Asclepias incarnata	Swamp Milkweed	2 GAL.	Heavy
		EC	Echinacea	Cone Flower	2 GAL	HEAVY
		CB	Cornus racemosa	Gray Dogwood	30"- 36" Ht.	Full & Dense
		CS	Cornus sericea	Red Twig Dogwood	30"- 36" Ht.	Full & Dense
		HA	Hamamelis virginiana	Common Witchhazel	3 GAL.	Full & Dense
		IVA	Ilex verticillata 'After Glow'	After Glow Winterberry	GAL.	Full & Dense
		IVJ	Ilex verticillata 'Jim Dandy'	Jim Dandy Winterberry	3'-4' Ht.	Full & Dense
		IG	Ilex glabra 'Compacta'	Compact Inkberry	2.5'-3' Ht.	B&B
		SS	Schizachyrium scoparium	Little bluestem	2 GAL.	Heavy
		MC	Muhlenbergia capillaris	Hairawn Muhly Grass	2 GAL.	Heavy
		PV	Panicum virgatum 'Heavy Metal'	Heavy Metal Switchgrass	2 GAL.	Heavy
		IV	Diervilla lonicera	Dwarf Bush-Honeysuckle	2 GAL.	Heavy
		SO	Solidago odora	Golden Rod	2 GAL.	Heavy
		BE	Rudbeckia hirta	Black-eyed Susan	2 GAL.	Heavy
		EP	Eupatorium perfoliatum	Common Boneset	2 GAL.	Heavy
		AE	Aster novae-anglicae	New England Aster	2 GAL.	Heavy
		CA	Carex amphibola	Creek Sedge	2 GAL.	Heavy

SITE NOTES

1. PATHWAY LOCATIONS SHOWN ARE PRELIMINARY IN NATURE AND SUBJECT TO FIELD STAKEOUT. IT IS THE INTENT OF THE PATHWAYS CONNECTING THE COTTAGES AND SITE ELEMENTS TO MEANDER UNDER EXISTING TREE CANOPY, WHERE FEASIBLE, BE PERVIOUS IN NATURE, BE FIELD LOCATED TO CREATE THE MINIMAL AMOUNT SITE DISTURBANCE POSSIBLE, TO AVOID CLEARING OF TREES GREATER THAN 12" IN DIAMETER, TO AVOID DISTURBANCE ANY CLOSER TO WETLANDS (AS SHOWN), AND MAINTAIN A GRADIENT OF LESS THAN 5% FOR UNIVERSAL ACCESSIBILITY.
2. COTTAGE LOCATIONS SHOWN ARE PRELIMINARY IN NATURE AND MAY VARY SLIGHTLY FROM THIS PLAN. IT IS THE INTENT OF THE FINAL COTTAGE LOCATIONS TO BE NO CLOSER TO WETLANDS (AS SHOWN) BUT BE STRATEGICALLY PLACED INTO THE LANDSCAPE ELEVATED ON PIERS, UNDER TREE CANOPY, TO AVOID UNNECESSARY TREE OR NON-INVASIVE VEGETATIVE UNDERSTORY CLEARING. FINAL COTTAGE LOCATIONS WILL BE STAKED OUT IN THE FIELD FOR BUILDING PERMIT REVIEW.

PLANTING NOTES

1. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO EXCAVATING PLANT PITS.
2. SEED ALL DISTURBED AREAS TO LAWN UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL PROVIDE A 6" MINIMUM DEPTH OF SCREENED TOPSOIL, AS SPECIFIED, FOR ALL LAWN AREAS. AS NOTED ON THE DETAILS, SUBGRADE BENEATH PROPOSED LAWN AREAS SHALL BE LOOSENED OR SCARIFIED TO A MINIMUM DEPTH OF 12 INCHES.
3. ALL PLANTING BEDS SHALL HAVE 12" MINIMUM DEPTH OF TOPSOIL.
4. THE CONTRACTOR SHALL PROVIDE A 4" MIN. DEPTH OF SHREDDED BARK MULCH OVER ALL PLANTING BEDS AND TREE PLANTINGS. MULCHED PLANT BEDS SHALL EXTEND 12" FURTHER THAN THE ADJACENT PLANTINGS. NO DYED MULCH.
5. ALL PLANT MATERIAL IS SUBJECT TO INSPECTION AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO AND AFTER PLANTING.
6. TAKE NOTE TO PROTECT ROOT ZONES OF EXISTING TREES ROOT ZONES DURING CONSTRUCTION AS SHOWN ON PLANS.

SEED MIX LEGEND

- (A) NEW ENGLAND WILD FLOWER MIX - ± 28,700 SF
  SEED RATE: 23 LB/ACRE 1900 SF/LB
- (B) NEW ENGLAND SEMI-SHADE GRASS & FORBS MIX - ± 72,500 SF
  SEED RATE: 30 LB/ACRE 1450 SF/LB
- (C) NEW ENGLAND CONSERVATION/WILDLIFE SEED MIX - ± 40,000 SF
  SEED RATE: 25 LB/ACRE 1750 SF/LB
- (D) NEW ENGLAND EROSION CONTROL/RESTORATION MIX FOR DETENTION BASINS AND MOIST SITES - ± 16,500 SF
  SEED RATE: 35 LB/ACRE 1250 SF/LB
- (E) NEW ENGLAND WETMIX (WETLAND SEED MIX) - ± 11,500 SF
  SEED RATE: 18 LB/ACRE 2500 SF/LB

AS MANUFACTURED BY:
NEW ENGLAND WETLAND PLANTS INC., AMHERST, MA
TEL. 413-548-8000
OR APPROVED EQUAL

EXISTING TREE LEGEND

- DEAD
- POOR
- FAIR
- GOOD

SLR
99 REALTY DRIVE
CHELMSFORD, MA 01410
(978) 271-1173
SLRCONSULTING.COM

DESCRIPTION	DATE	BY
PAZ SUBMISSION	8/1/2024	SB
PEER REVIEW COMMENTS	9/9/2024	SB
TOWN COMMENTS	11/6/2024	SB
PEER REVIEW COMMENTS	11/26/2024	SB
PUBLIC HEARING COMMENTS	12/9/2024	TDR

SITE PLAN - LANDSCAPING

WAKE ROBIN INN REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

DESIGNED	DRAWN	CHECKED
MA	SB	MA

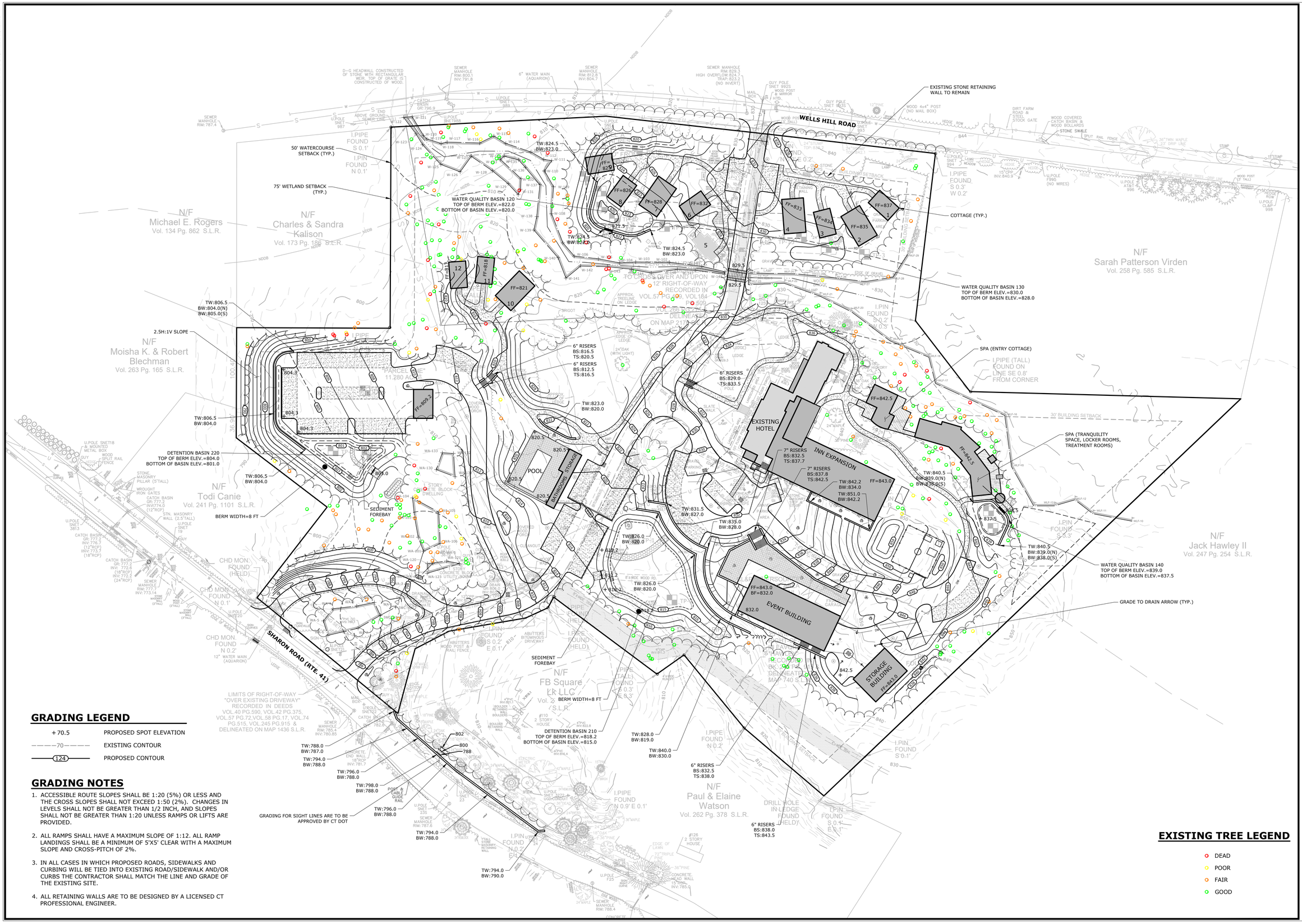
SCALE: 1"=50'

DATE: JULY 29, 2024

PROJECT NO.: 22100.00001

SHEET NO.: 05 OF 23

SHEET NAME: LS



GRADING LEGEND

+70.5	PROPOSED SPOT ELEVATION
———70———	EXISTING CONTOUR
——(124)——	PROPOSED CONTOUR

GRADING NOTES

1. ACCESSIBLE ROUTE SLOPES SHALL BE 1:20 (5%) OR LESS AND THE CROSS SLOPES SHALL NOT EXCEED 1:50 (2%). CHANGES IN LEVELS SHALL NOT BE GREATER THAN 1/2 INCH, AND SLOPES SHALL NOT BE GREATER THAN 1:20 UNLESS RAMPS OR LIFTS ARE PROVIDED.
2. ALL RAMPS SHALL HAVE A MAXIMUM SLOPE OF 1:12. ALL RAMP LANDINGS SHALL BE A MINIMUM OF 5'X5' CLEAR WITH A MAXIMUM SLOPE AND CROSS-PITCH OF 2%.
3. IN ALL CASES IN WHICH PROPOSED ROADS, SIDEWALKS AND CURBING WILL BE TIED INTO EXISTING ROAD/SIDEWALK AND/OR CURBS THE CONTRACTOR SHALL MATCH THE LINE AND GRADE OF THE EXISTING SITE.
4. ALL RETAINING WALLS ARE TO BE DESIGNED BY A LICENSED CT PROFESSIONAL ENGINEER.

EXISTING TREE LEGEND

- DEAD
- POOR
- FAIR
- GOOD

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DESCRIPTION	DATE	BY
P&Z SUBMISSION	8/1/2024	SM
PEER REVIEW COMMENTS	8/9/2024	DSR
TOWN COMMENTS	11/6/2024	SM
PEER REVIEW COMMENTS	11/26/2024	SM
PUBLIC HEARING COMMENTS	12/9/2024	TDR

SITE PLAN - GRADING

WAKE ROBIN INN REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	MA
DESIGNED	DRAWN	CHECKED

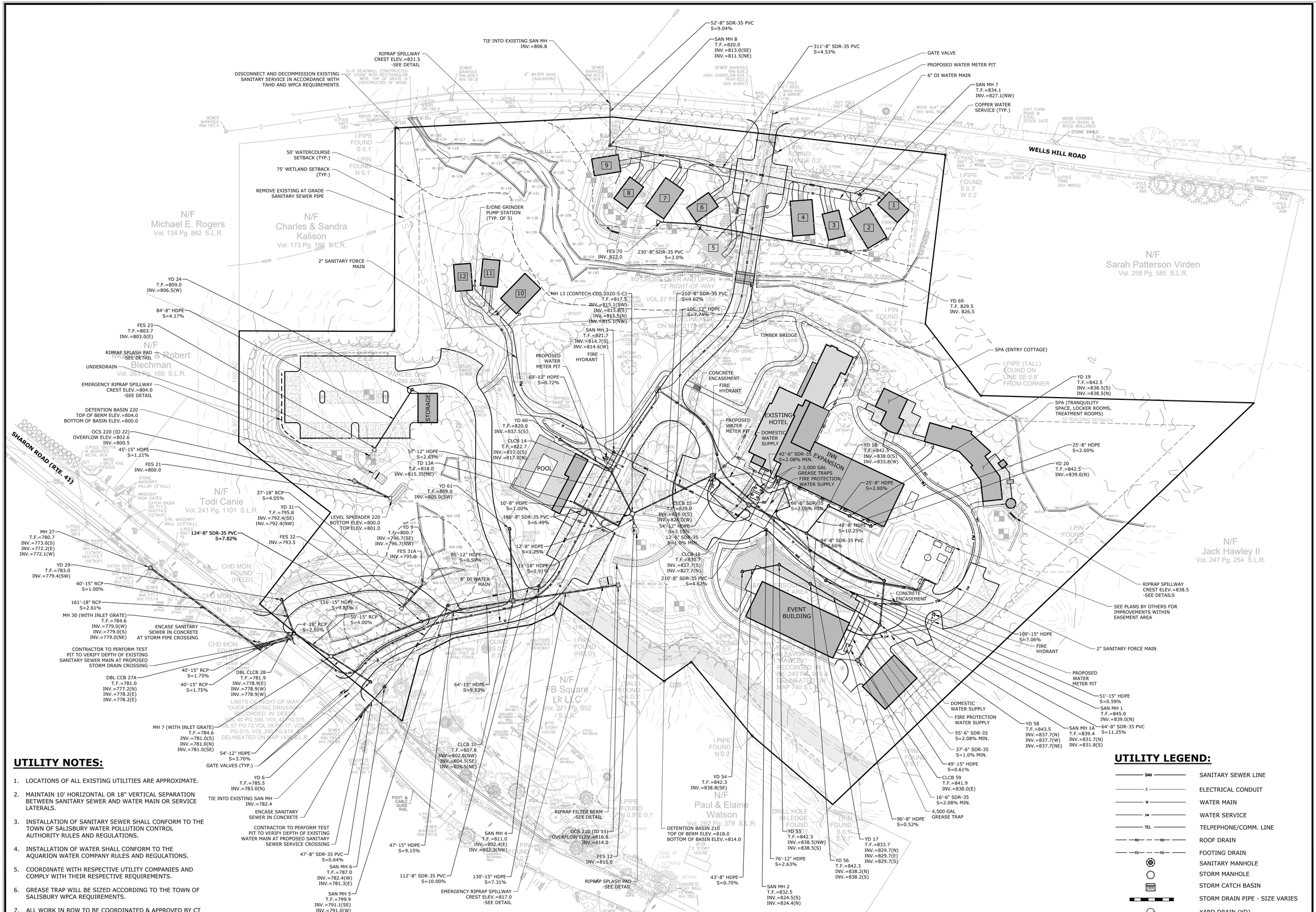
1"=50'
SCALE

JULY 29, 2024
DATE

22100.00001
PROJECT NO.

06 OF 23
SHEET NO.

GR
SHEET NAME

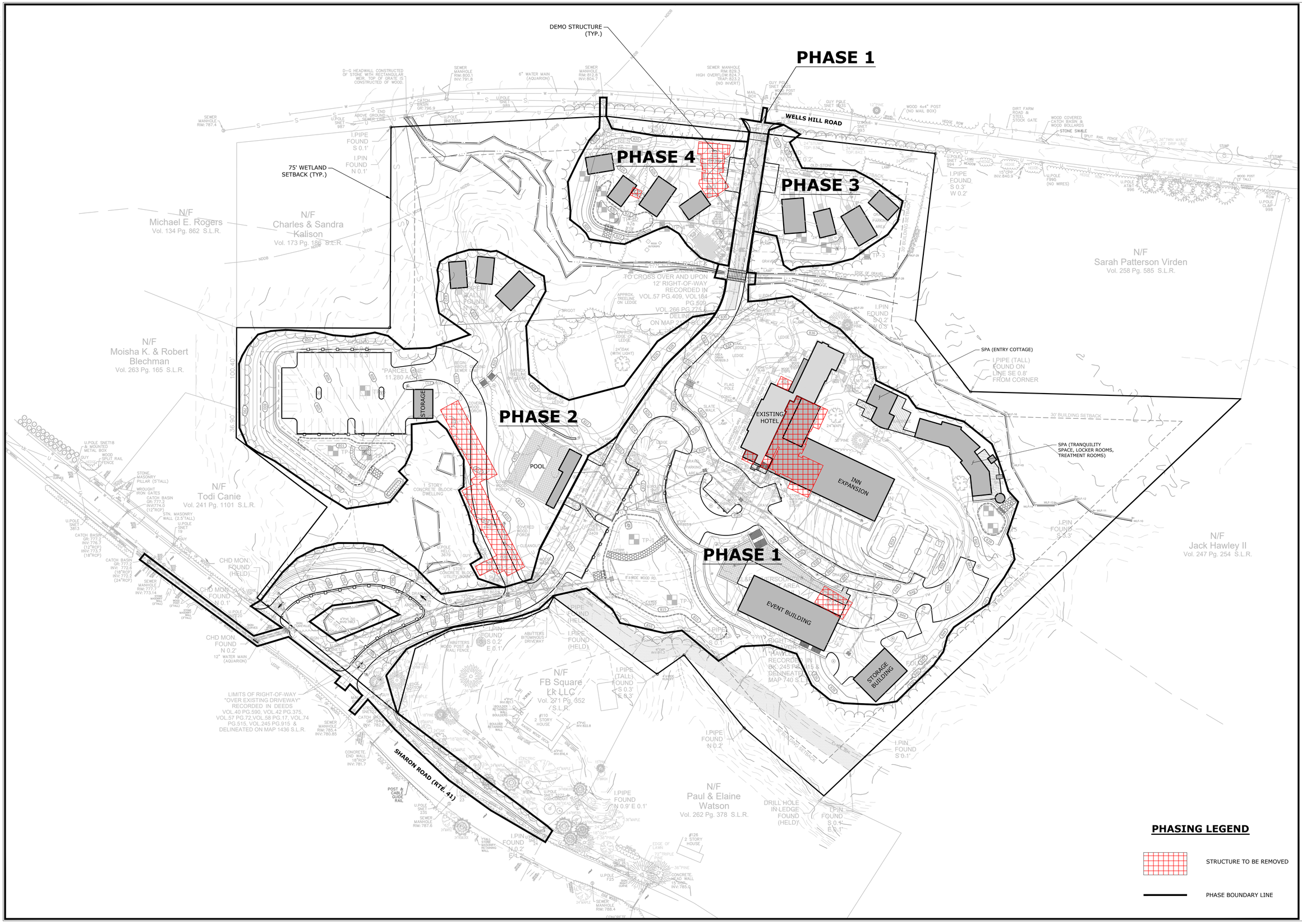


UTILITY NOTES:

1. LOCATIONS OF ALL EXISTING UTILITIES ARE APPROXIMATE.
2. MAINTAIN 10' HORIZONTAL OR 18" VERTICAL SEPARATION BETWEEN SANITARY SEWER AND WATER MAIN OR SERVICE LATERALS.
3. INSTALLATION OF SANITARY SEWER SHALL CONFORM TO THE TOWN OF SALISBURY WATER POLLUTION CONTROL AUTHORITY RULES AND REGULATIONS.
4. INSTALLATION OF WATER SHALL CONFORM TO THE AQUARION WATER COMPANY RULES AND REGULATIONS.
5. COORDINATE WITH RESPECTIVE UTILITY COMPANIES AND COMPLY WITH THEIR RESPECTIVE REQUIREMENTS.
6. GREASE TRAP WILL BE SIZED ACCORDING TO THE TOWN OF SALISBURY WPCA REQUIREMENTS.
7. ALL WORK IN ROW TO BE COORDINATED & APPROVED BY CT DOT.

UTILITY LEGEND:

- SAN — SANITARY SEWER LINE
- E — ELECTRICAL CONDUIT
- W — WATER MAIN
- DW — WATER SERVICE
- TEL — TELPEPHONE/COMM. LIN



0' 25' 50'
0 1/2" 1"

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DESCRIPTION	DATE	BY
PEER REVIEW COMMENTS	9/6/2024	DSR
TOWN COMMENTS	11/6/2024	SM
PUBLIC HEARING COMMENTS	12/9/2024	TDR

PHASING PLAN

WAKE ROBIN INN
REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	TR
DESIGNED	DRAWN	CHECKED

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PP-1
SHEET NAME

CONSTRUCTION SEQUENCE - PHASING PLAN

GENERAL NOTES

1. AT LEAST SIXTY DAYS PRIOR TO THE START OF CONSTRUCTION THE DEVELOPER IS TO SUBMIT TO THE STATE OF CONNECTICUT DEPARTMENT OF ENERGY AND ENVIRONMENTAL PROTECTION (CTDEEP) A COMPLETED GENERAL PERMIT REGISTRATION FORM FOR THE DISCHARGE OF STORMWATER AND DEWATERING WASTEWATERS FROM CONSTRUCTION ACTIVITIES. AFTER THE DEPARTMENT'S REVIEW, THE DEVELOPER WILL MAKE THE NECESSARY PLAN CHANGES PRIOR TO THE START OF CONSTRUCTION.
2. SEDIMENT AND EROSION CONTROL INSPECTION REPORTS SHALL BE COMPLETED THROUGHOUT CONSTRUCTION WITH COPIES MAINTAINED ONSITE AND ALSO SUBMITTED TO CTDEEP AND THE TOWN OF SALISBURY LAND USE AGENCY, IN ACCORDANCE WITH STATE AND LOCAL PERMIT REQUIREMENTS.
3. AT THIS TIME A PRE-CONSTRUCTION MEETING SHALL BE HELD BETWEEN THE DEVELOPER, TOWN STAFF, AND DESIGN ENGINEER. AT THIS MEETING SOMEONE WILL BE NAMED RESPONSIBLE FOR MAINTAINING THE SEDIMENT AND EROSION CONTROL MEASURES. EROSION CONTROL INSPECTIONS SHALL BE PERFORMED ON A WEEKLY BASIS BY THIS PERSON AND AN INSPECTION REPORT BE SUBMITTED TO TOWN STAFF. AREAS WHERE THE EROSION CONTROL SYSTEMS HAVE FAILED SHALL BE NOTED AND SHALL BE REPAIRED PROMPTLY. A LOG OF ALL INSPECTIONS AND A COPY OF THE CURRENT DESIGN PLANS SHALL BE KEPT ON SITE AND BE AVAILABLE FOR VIEWING.
4. THE SOIL EROSION AND SEDIMENT CONTROLS SHALL BE MODIFIED BY THE CONTRACTOR AT THE DIRECTION OF THE ENGINEER AND/OR A DESIGNATED TOWN REPRESENTATIVE AS NECESSITATED BY CHANGING SITE CONDITIONS.
5. THE SITE SHOULD BE KEPT CLEAN OF LOOSE DEBRIS, LITTER, AND BUILDING MATERIALS SUCH THAT NONE OF THE ABOVE ENTERS WETLANDS OR WATERCOURSES.
6. THE FOLLOWING IS INTENDED TO OUTLINE A REASONABLE CONSTRUCTION SEQUENCE OF MAJOR TASKS THAT MINIMIZES THE AMOUNT OF EXPOSED SOIL AREA AT ANY ONE TIME. THE AMOUNT OF EXPOSED SOIL SHALL BE LIMITED TO ACTIVE WORK AREAS ONLY AND BE KEPT TO A MINIMUM AT ALL TIMES. THE BEST WAY TO MINIMIZE SOIL EROSION IS TO MAINTAIN VEGETATIVE COVER AND KEEP DISTURBED AREAS BELOW FIVE ACRES DRAINING TO ANY ONE LOCATION. VEGETATIVE COVER WHETHER TEMPORARY OR PERMANENT SHALL BE ESTABLISHED AS SOON AS POSSIBLE. ANY CHANGES TO THE SEQUENCE OF CONSTRUCTION MUST BE COORDINATED WITH THE TOWN ENGINEER AND/OR A DESIGNATED TOWN REPRESENTATIVE. THE SELECTED SITE CONTRACTOR SHALL REVIEW THE EROSION AND SEDIMENT CONTROL PLANS AND SUBMIT A FINAL PLAN, CONSTRUCTION SEQUENCE, AND SCHEDULE PRIOR TO INITIATION OF EACH PHASE. SUCH PLAN AND SUPPORTING INFORMATION SHALL BE PREPARED BY A PROFESSIONAL ENGINEER OR CERTIFIED PROFESSIONAL IN EROSION AND SEDIMENT CONTROL.
7. ALL SEDIMENT AND EROSION CONTROLS SHALL BE CONSISTENT WITH THE 2024 CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL AND SALISBURY PLANNING & ZONING REGULATIONS.
8. IN ORDER TO MINIMIZE THE AMOUNT OF EXPOSED SOIL AREA THE CONSTRUCTION SEQUENCE HAS BEEN DIVIDED INTO FOUR PHASES.
9. BITUMINOUS CONCRETE TOP COURSE FOR MAIN CORRIDOR WHICH CONNECTS SHARON ROAD AND WELLS HILL ROAD WILL BE COMPLETED LAST.
10. UPON COMPLETION OF THE FINAL PHASE OF CONSTRUCTION AND FINAL EROSION CONTROL INSPECTION IN ACCORDANCE WITH DEEP GENERAL PERMIT, THE REGISTRANT SHALL FILE A NOTICE OF TERMINATION TO CLOSE THE PERMIT.

PRE-CONSTRUCTION & DEMOLITION PHASE

1. CONTRACTOR TO STAKE OUT LIMIT OF DISTURBANCE FOR CONSTRUCTION FOR PHASES 1 THRU 4. NO DISTURBANCE IS TO TAKE PLACE BEYOND THE LIMITS OF WORK SHOWN.
2. INSTALL CONSTRUCTION ENTRANCES, INLET PROTECTION AND PERIMETER EROSION CONTROLS AS DEPICTED ON THE SOIL EROSION AND SEDIMENT CONTROL PLANS.
3. BEGIN TREE CLEARING OPERATIONS FOR PHASES PHASES 1 THRU 4. REMOVE STUMPS LOCATED WITHIN THE CLEARED AREA. ANY PORTION OF THE CLEARED AREA THAT WILL NOT BE ACTIVE WITHIN ONE MONTH SHALL BE STABILIZED WITH HAY AND SEED AFTER STUMPS ARE REMOVED. STUMPS ARE TO BE GROUND INTO MULCH OR REMOVED AND DISPOSED OF OFF-SITE. WOODCHIPS FROM CLEARING OPERATIONS MAY BE STOCKPILED TO BE USED FOR EROSION CONTROL DURING THE WINTER MONTHS TO BLANKET DISTURBED AREAS WHEN TURF ESTABLISHMENT IS IMPRACTICAL.
4. CONDUCT DEMOLITION OF ANY EXISTING STRUCTURES OR OTHER EXISTING IMPROVEMENTS THAT ARE DESIGNATED TO BE REMOVED FOR PHASES 1 THRU 4. ALL TRASH AND OTHER SURFACE DEBRIS SHOULD ALSO BE REMOVED AT THIS TIME AND DISPOSED OF AT AN APPROPRIATE OFF-SITE FACILITY.
5. NO WORK SHALL PROCEED ON PHASE 1 UNTIL AUTHORIZED BY THE TOWN LAND USE AGENCY.

PHASE 1 CONSTRUCTION

1. CONSTRUCT DRAINAGE SWALES, DIVERSION BERMS AND TEMPORARY SEDIMENT TRAPS/BASINS FOR PHASE 1. TEMPORARY SEDIMENT TRAPS/BASINS ARE TO BE CONSTRUCTED FIRST. THE BOTTOM OF THE TRAPS ARE TWO FEET HIGHER THAN THE BOTTOM OF THE PROPOSED BASINS. ONCE THE TRAP IS STABILIZED, CONSTRUCTION MAY BE COMMENCED FOR HOTEL BUILDING ADDITION, EVENT BARN, SPA, STORAGE BUILDING, ROADWAYS, AND UTILITIES.
2. BEGIN STRIPPING TOPSOIL FOR THE ROADWAYS, FOLLOWED BY THE BUILDING LOCATIONS. TOPSOIL SHALL BE STOCKPILED WITHIN LIMITS OF CLEARING DESIGNATED ON THE DESIGN PLANS AND BE ENCIRCLED WITH SEDIMENT FILTER FENCE. TOPSOIL STOCKPILES THAT ARE TO SIT UNDISTURBED FOR GREATER THAN THIRTY DAYS ARE TO BE STABILIZED WITH TEMPORARY SEEDING.
3. CONSTRUCT STORM DRAINS, UTILITIES, RETAINING WALLS, ROADWAYS AND PARKING AREAS ASSOCIATED WITH PHASE 1. INSTALL INLET PROTECTION FOR INSTALLED CATCH BASINS AND YARD DRAINS.
4. BEGIN MASS EARTHWORK FOR THE MAIN DRIVEWAY ENTRANCE SIGHT LINES, NEW BUILDINGS, PARKING AREAS AND TRAILS ASSOCIATED WITH PHASE 1. ANY BLASTING REQUIRED SHALL BE PERFORMED ACCORDING TO THE TOWN OF SALISBURY STANDARDS AND APPLICABLE INDUSTRY STANDARDS. ALL BLASTING SHALL BE COORDINATED WITH THE TOWN OF SALISBURY FIRE MARSHAL.
5. ONCE ROUGH GRADE IS REACHED ALL STORM DRAINAGE AND UTILITY SERVICE INSTALLATIONS SHALL BE COMPLETED. EXCESS EXCAVATED SOIL MATERIAL FROM PHASE 1 SHALL BE DEPOSITED AT SPECIFIED FILL LOCATIONS IN OTHER PHASES.
6. WHEN BUILDING CONSTRUCTION, UTILITY SERVICE INSTALLATION AND TRAILS ARE COMPLETE, TOPSOIL SHALL BE PLACED AND FINE GRADED TO FINISHED GRADE SHOWN ON SITE PLANS. PERMANENT SEEDING, LANDSCAPE PLANTINGS AND IRRIGATION SHALL BE COMPLETED AT THIS TIME, ALONG WITH INSTALLATION OF THE BASE LAYER OF BITUMINOUS CONCRETE PAVEMENT.
7. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE UNTIL ALL NEW BUILDINGS ARE CONSTRUCTED AND THE SITE IS PERMANENTLY STABILIZED UP SLOPE OF THE PERIMETER EROSION CONTROL.
8. UPON COMPLETION OF ALL BUILDINGS IN PHASE 1 AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS ASSOCIATED WITH CONSTRUCTION, TEMPORARY SEDIMENT BASIN #2 SHALL BE CONVERTED TO PERMANENT DETENTION BASIN 210 AND ALL STORM DRAINAGE STRUCTURES WITHIN PHASE 1 SHALL BE INSPECTED AND THOROUGHLY CLEANED OF ACCUMULATED SEDIMENT AND DEBRIS.
9. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE DOWN SLOPE OF ALL DISTURBED SITE AREAS UNTIL THE SITE IS PERMANENTLY STABILIZED.
10. NO WORK SHALL PROCEED ON PHASE 2 UNTIL AUTHORIZED BY THE TOWN LAND USE AGENCY.

PHASE 2 CONSTRUCTION

1. CONSTRUCT DRAINAGE SWALES, DIVERSION BERMS AND TEMPORARY SEDIMENT TRAPS/BASINS FOR PHASE 2. TEMPORARY SEDIMENT TRAPS/BASINS ARE TO BE CONSTRUCTED FIRST. THE BOTTOM OF THE TRAPS ARE TWO FEET HIGHER THAN THE BOTTOM OF THE PROPOSED BASINS. ONCE THE TRAP IS STABILIZED, CONSTRUCTION MAY BE COMMENCED FOR THE POOL, COTTAGES, STORAGE BUILDING, ROADWAYS, AND UTILITIES.
2. BEGIN STRIPPING TOPSOIL FOR THE ROADWAYS, FOLLOWED BY THE BUILDING LOCATIONS. TOPSOIL SHALL BE STOCKPILED WITHIN LIMITS OF CLEARING DESIGNATED ON THE DESIGN PLANS AND BE ENCIRCLED WITH SEDIMENT FILTER FENCE. TOPSOIL STOCKPILES THAT ARE TO SIT UNDISTURBED FOR GREATER THAN THIRTY DAYS ARE TO BE STABILIZED WITH TEMPORARY SEEDING.
3. CONSTRUCT STORM DRAINS, UTILITIES, RETAINING WALLS, ROADWAYS AND PARKING AREAS ASSOCIATED WITH PHASE 2. INSTALL INLET PROTECTION FOR INSTALLED CATCH BASINS AND YARD DRAINS.
4. BEGIN MASS EARTHWORK FOR THE NEW BUILDINGS, ROADWAYS, PARKING AREAS AND TRAILS ASSOCIATED WITH PHASE 2. ANY BLASTING REQUIRED SHALL BE PERFORMED ACCORDING TO THE TOWN OF SALISBURY STANDARDS AND APPLICABLE INDUSTRY STANDARDS. ALL BLASTING SHALL BE COORDINATED WITH THE TOWN OF SALISBURY FIRE MARSHAL.
5. ONCE ROUGH GRADE IS REACHED ALL STORM DRAINAGE AND UTILITY SERVICE INSTALLATIONS SHALL BE COMPLETED. EXCESS EXCAVATED SOIL MATERIAL FROM PHASE 2 SHALL BE DEPOSITED AT SPECIFIED FILL LOCATIONS IN OTHER PHASES.
6. WHEN BUILDING CONSTRUCTION AND UTILITY SERVICE INSTALLATION IS COMPLETE, TOPSOIL SHALL BE PLACED AND FINE GRADED TO FINISHED GRADE SHOWN ON SITE PLANS. PERMANENT SEEDING, LANDSCAPE PLANTINGS AND IRRIGATION SHALL BE COMPLETED AT THIS TIME, ALONG WITH INSTALLATION OF THE BASE LAYER OF BITUMINOUS CONCRETE PAVEMENT.
7. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE UNTIL ALL NEW BUILDINGS ARE CONSTRUCTED AND THE SITE IS PERMANENTLY STABILIZED UP SLOPE OF THE PERIMETER EROSION CONTROL.
8. UPON COMPLETION OF ALL BUILDINGS IN PHASE 2 AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS ASSOCIATED WITH CONSTRUCTION, TEMPORARY SEDIMENT BASIN #1 SHALL BE CONVERTED TO PERMANENT DETENTION BASIN 220 AND ALL STORM DRAINAGE STRUCTURES WITHIN PHASE 2 SHALL BE INSPECTED AND THOROUGHLY CLEANED OF ACCUMULATED SEDIMENT AND DEBRIS.
9. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE DOWN SLOPE OF ALL DISTURBED SITE AREAS UNTIL THE SITE IS PERMANENTLY STABILIZED.
10. NO WORK SHALL PROCEED ON PHASE 3 UNTIL AUTHORIZED BY THE TOWN LAND USE AGENCY.

PHASE 3 CONSTRUCTION

1. BEGIN STRIPPING TOPSOIL FOR THE PARKING AREA FOLLOWED BY THE COTTAGE LOCATIONS. TOPSOIL SHALL BE STOCKPILED WITHIN LIMITS OF CLEARING DESIGNATED ON THE DESIGN PLANS AND BE ENCIRCLED WITH SEDIMENT FILTER FENCE. TOPSOIL STOCKPILES THAT ARE TO SIT UNDISTURBED FOR GREATER THAN THIRTY DAYS ARE TO BE STABILIZED WITH TEMPORARY SEEDING.
2. BEGIN MASS EARTHWORK FOR THE NEW COTTAGES, PARKING AREAS WATER QUALITY BASIN AND TRAILS ASSOCIATED WITH PHASE 3. ANY BLASTING REQUIRED SHALL BE PERFORMED ACCORDING TO THE TOWN OF SALISBURY STANDARDS AND APPLICABLE INDUSTRY STANDARDS. ALL BLASTING SHALL BE COORDINATED WITH THE TOWN OF SALISBURY FIRE MARSHAL.
3. CONSTRUCT STORM DRAINS, UTILITIES, ROADWAYS AND PARKING AREAS ASSOCIATED WITH PHASE 3.
4. ONCE ROUGH GRADE IS REACHED ALL STORM DRAINAGE AND UTILITY SERVICE INSTALLATIONS SHALL BE COMPLETED. EXCESS EXCAVATED SOIL MATERIAL FROM PHASE 3 SHALL BE DEPOSITED IN PHASE 4.
5. WHEN BUILDING CONSTRUCTION AND UTILITY SERVICE INSTALLATION ARE COMPLETE, TOPSOIL SHALL BE PLACED AND FINE GRADED TO FINISHED GRADE SHOWN ON SITE PLANS. PERMANENT SEEDING, LANDSCAPE PLANTINGS AND IRRIGATION SHALL BE COMPLETED AT THIS TIME.
6. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE UNTIL ALL NEW BUILDINGS ARE CONSTRUCTED AND THE SITE IS PERMANENTLY STABILIZED UP SLOPE OF THE PERIMETER EROSION CONTROL.
7. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE DOWN SLOPE OF ALL DISTURBED SITE AREAS UNTIL THE SITE IS PERMANENTLY STABILIZED.
8. NO WORK SHALL PROCEED ON PHASE 4 UNTIL AUTHORIZED BY THE TOWN LAND USE AGENCY.

PHASE 4 CONSTRUCTION

1. BEGIN STRIPPING TOPSOIL FOR THE PARKING AREA FOLLOWED BY THE COTTAGE LOCATIONS. TOPSOIL SHALL BE STOCKPILED WITHIN LIMITS OF CLEARING DESIGNATED ON THE DESIGN PLANS AND BE ENCIRCLED WITH SEDIMENT FILTER FENCE OR EXPORTED FROM THE SITE. TOPSOIL STOCKPILES THAT ARE TO SIT UNDISTURBED FOR GREATER THAN THIRTY DAYS ARE TO BE STABILIZED WITH TEMPORARY SEEDING.
2. BEGIN MASS EARTHWORK FOR THE NEW COTTAGES, PARKING AREA, WATER QUALITY BASIN AND TRAILS ASSOCIATED WITH PHASE 4. ANY BLASTING REQUIRED SHALL BE PERFORMED ACCORDING TO THE TOWN OF SALISBURY STANDARDS AND APPLICABLE INDUSTRY STANDARDS. ALL BLASTING SHALL BE COORDINATED WITH THE TOWN OF SALISBURY FIRE MARSHAL.
3. CONSTRUCT STORM DRAINS, UTILITIES, RETAINING WALLS, ROADWAYS AND PARKING AREAS ASSOCIATED WITH PHASE 4.
4. ONCE ROUGH GRADE IS REACHED ALL STORM DRAINAGE AND UTILITY SERVICE INSTALLATIONS SHALL BE COMPLETED. EXCESS EXCAVATED SOIL MATERIAL FROM PHASE 4 SHALL BE REMOVED FROM THE SITE.
5. WHEN BUILDING AND UTILITY SERVICE INSTALLATION ARE COMPLETE, TOPSOIL SHALL BE PLACED AND FINE GRADED TO FINISHED GRADE SHOWN ON SITE PLANS. PERMANENT SEEDING, LANDSCAPE PLANTINGS AND IRRIGATION SHALL BE COMPLETED AT THIS TIME.
6. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE UNTIL THE NEW BUILDINGS ARE CONSTRUCTED AND THE SITE IS PERMANENTLY STABILIZED UP SLOPE OF THE PERIMETER EROSION CONTROL.
7. PERIMETER EROSION CONTROLS ARE TO REMAIN IN PLACE DOWN SLOPE OF ALL DISTURBED SITE AREAS UNTIL THE SITE IS PERMANENTLY STABILIZED.

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DESCRIPTION	DATE	BY
TOWN COMMENTS	11/8/2024	DSR
TOWN COMMENTS	11/6/2024	SM

PHASING PLAN NOTES

WAKE ROBIN INN
REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

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PP-2
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TEMPORARY SEDIMENT TRAP SIZING SUMMARY

TRAP NO.	ACRES	VOLUME STORAGE REQUIRED	DEPTH STORAGE REQUIRED	ACRES/ DIMENSIONS	VOLUME STORAGE PROPOSED
#1	1.0	134 CY	2.5 FT.	3,700 SF	340 CY**
#2	2.3	308 CY	1.5 FT.	11,700 SF	650 CY**

*134 CY STORAGE VOLUME REQUIRED PER ACRE CONTRIBUTING AREA TO TST
**ONE HALF WET-STORAGE, ONE HALF DRY-STORAGE

SOIL EROSION AND SEDIMENT CONTROL NARRATIVE

SEDIMENT AND EROSION CONTROL MEASURES AS DEPICTED ON THESE PLANS AND DESCRIBED WITHIN THE SEDIMENT AND EROSION CONTROL NARRATIVE SHALL BE IMPLEMENTED AND MAINTAINED UNTIL PERMANENT COVER AND STABILIZATION IS ESTABLISHED. ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL CONFORM TO THE "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL - 2024, TOWN OF SALISBURY REQUIREMENTS, AND IN ALL CASES BEST MANAGEMENT PRACTICES SHALL PREVAIL.

1.PURPOSE AND DESCRIPTION OF PROJECT
A.) THE CONSTRUCTION OF A WEDDING/EVENTS VENUE WITH COMMON AMENITY AREAS ALONG WITH DRIVEWAY, UTILITY, AND STORMWATER IMPROVEMENTS.
B.) DISTURBED AREA: ±8.4 AC

2.IDENTIFICATION OF EROSION AND SEDIMENT CONTROL CONCERNS
A.) CUTS AND FILLS ASSOCIATED WITH CONSTRUCTION.
B.) PROTECTION OF WETLANDS/WATERCOURSES ON SITE.

3.IDENTIFICATION OF OTHER POSSIBLE PERMITS
THE PERMITS REQUIRED FOR THE PROJECT INCLUDE, BUT ARE NOT LIMITED TO: LOCAL PLANNING AND ZONING AND INLAND WETLANDS PERMITS, DEEP STORMWATER GENERAL PERMIT, AND DOT ENCROACHMENT PERMIT.

4. PARTY RESPONSIBLE FOR SEDIMENT AND EROSION CONTROLS
TO BE DETERMINED PRIOR TO START OF CONSTRUCTION IN ACCORDANCE WITH STORMWATER GENERAL PERMIT AND OTHER APPLICABLE PERMIT REQUIREMENTS.

EROSION CONTROL LEGEND

- CE — CONSTRUCTION ENTRANCE ANTI-TRACKING PAD
- SW — STRAW WATTLE
- SF — SEDIMENT FILTER FENCE
- TST — TEMPORARY SEDIMENT TRAP
- TEMPORARY CONTOUR FOR TST
- NRCS SOILS BOUNDARY
- IP — INLET PROTECTION
- STK — SOIL STOCKPILE AREA
- DB — TEMPORARY DIVERSION BERM/SWALE WITH CHECK DAMS
- RIP RAP OVERFLOW DISCHARGE
- ECB — EROSION CONTROL BLANKET
- 45B — NRCS SOILS BOUNDARY TYPE

NRCS SOIL TYPES

8: MUDGEPOND AND ALDEN SOILS, EXTREMELY STONY
48B: GEORGIA AND AMENIA SILT LOAMS, 2-8% SLOPES
80B: BERNARDSTON SILT LOAM, 3-8% SLOPES
90C: STOCKBRIDGE LOAM, 8-15% SLOPES
94C: FARMINGTON-NELLIS COMPLEX, 3-15% SLOPES
95E: FARMINGTON-ROCK OUTCROP COMPLEX, 15-45% SLOPES

0' 25' 50'
0 1/2" 1"

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SEDIMENT & EROSION CONTROL PLAN

WAKE ROBIN INN
REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

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SEDIMENT & EROSION CONTROL SPECIFICATIONS

GENERAL:

THESE GUIDELINES SHALL APPLY TO ALL WORK CONSISTING OF ANY AND ALL TEMPORARY AND/OR PERMANENT MEASURES TO CONTROL WATER POLLUTION AND SOIL EROSION, AS MAY BE REQUIRED, DURING THE CONSTRUCTION OF THE PROJECT.

IN GENERAL, ALL CONSTRUCTION ACTIVITIES SHALL PROCEED IN SUCH A MANNER SO AS NOT TO POLLUTE ANY WETLANDS, WATERCOURSE, WATER BODY, AND CONDUIT CARRYING WATER, ETC. THE CONTRACTOR SHALL LIMIT, INSOFAR AS POSSIBLE, THE SURFACE AREA OF EARTH MATERIALS EXPOSED BY CONSTRUCTION METHODS AND IMMEDIATELY PROVIDE PERMANENT AND TEMPORARY POLLUTION CONTROL MEASURES TO PREVENT CONTAMINATION OF ADJACENT WETLANDS, WATERCOURSES, AND WATER BODIES, AND TO PREVENT, INSOFAR AS POSSIBLE, EROSION ON THE SITE.

LAND GRADING:

THE RESHAPING OF THE GROUND SURFACE BY EXCAVATION AND FILLING OR A COMBINATION OF BOTH, TO OBTAIN PLANNED GRADES, SHALL PROCEED IN ACCORDANCE WITH THE FOLLOWING CRITERIA:

a. THE CUT FACE OF EARTH EXCAVATION SHALL NOT BE STEEPER THAN TWO HORIZONTAL TO ONE VERTICAL (2:1).

b. THE PERMANENT EXPOSED FACES OF FILLS SHALL NOT BE STEEPER THAN TWO HORIZONTAL TO ONE VERTICAL (2:1).

c. THE CUT FACE OF ROCK EXCAVATION SHALL NOT BE STEEPER THAN ONE HORIZONTAL TO TWO VERTICAL (1:2).

d. PROVISIONS SHOULD BE INCLUDED TO CONVEY SURFACE WATER SAFELY TO STORM DRAINS TO PREVENT SURFACE RUNOFF FROM DAMAGING CUT FACES AND FILL SLOPES.

e. NO FILL SHOULD BE PLACED WHERE IT WILL SLIDE OR WASH UPON THE INTO ADJACENT WETLANDS, WATERCOURSES, OR WATER BODIES.

f. PRIOR TO ANY RE-GRADING, A STABILIZED CONSTRUCTION ENTRANCE SHALL BE PLACED AT THE ENTRANCE TO THE WORK AREA IN ORDER TO REDUCE MUD AND OTHER SEDIMENTS FROM LEAVING THE SITE.

TOPSOILING:

TOPSOIL SHALL BE SPREAD OVER ALL EXPOSED AREAS IN ORDER TO PROVIDE A SOIL MEDIUM HAVING FAVORABLE CHARACTERISTICS FOR THE ESTABLISHMENT, GROWTH, AND MAINTENANCE OF VEGETATION.

UPON ATTAINING FINAL SUBGRADES, SCARIFY SURFACE TO PROVIDE A GOOD BOND WITH TOPSOIL.

REMOVE ALL LARGE STONES, TREE LIMBS, ROOTS AND CONSTRUCTION DEBRIS.

APPLY LIME ACCORDING TO SOIL TEST OR AT THE RATE OF TWO (2) TONS PER ACRE.

MATERIAL:

1. TOPSOIL SHOULD HAVE PHYSICAL, CHEMICAL, AND BIOLOGICAL CHARACTERISTICS FAVORABLE TO THE GROWTH OF PLANTS.
2. TOPSOIL SHOULD HAVE A SANDY OR LOAMY TEXTURE.
3. TOPSOIL SHOULD BE RELATIVELY FREE OF SUBSOIL MATERIAL AND MUST BE FREE OF STONES (OVER 1" IN DIAMETER), LUMPS OF SOIL, ROOTS, TREE LIMBS, TRASH, OR CONSTRUCTION DEBRIS. IT SHOULD BE FREE OF ROOTS OR RHIZOMES SUCH AS THISTLE, NUTGRASS, AND QUACKGRASS.
4. AN ORGANIC MATTER CONTENT OF SIX PERCENT (6%) IS REQUIRED. AVOID LIGHT COLORED SUBSOIL MATERIAL.
5. SOLUBLE SALT CONTENT OF OVER 500 PARTS PER MILLION (PPM) IS LESS SUITABLE. AVOID TIDAL MARSH SOILS BECAUSE OF HIGH SALT CONTENT AND SULFUR ACIDITY.
6. THE pH SHOULD BE MORE THAN 6.0. IF LESS, ADD LIME TO INCREASE pH TO AN ACCEPTABLE LEVEL.

APPLICATION:

1. AVOID SPREADING WHEN TOPSOIL IS WET OR FROZEN.
2. SPREAD TOPSOIL UNIFORMLY TO A DEPTH OF AT LEAST SIX INCHES (6"), OR TO THE DEPTH SHOWN ON THE PLANS.

TEMPORARY VEGETATIVE COVER:

TEMPORARY VEGETATIVE COVER SHALL BE ESTABLISHED ON ALL UNPROTECTED AREAS THAT PRODUCE SEDIMENT, AREAS WHERE FINAL GRADING HAS BEEN COMPLETED, AND AREAS WHERE THE ESTIMATED PERIOD OF BARE SOIL EXPOSURE IS LESS THAN 12 MONTHS. TEMPORARY VEGETATIVE COVER SHALL BE APPLIED IF AREAS WILL NOT BE PERMANENTLY SEEDED BY SEPTEMBER 1.

SITE PREPARATION:

1. INSTALL REQUIRED SURFACE WATER CONTROL MEASURES.
2. REMOVE LOOSE ROCK, STONE, AND CONSTRUCTION DEBRIS FROM AREA.
3. APPLY LIME ACCORDING TO SOIL TEST OR AT A RATE OF ONE (1) TON OF GROUND DOLOMITIC LIMESTONE PER ACRE (5 LBS. PER 100 SQ. FT.).
4. APPLY FERTILIZER ACCORDING TO SOIL TEST OR AT THE RATE OF 300 LBS. OF 10-10-10 PER ACRE (7 LBS. PER 1,000 SQ. FT.) AND SECOND APPLICATION OF 200 LBS. OF 10-10-10- (5 LBS. PER 1,000 SQ. FT.) WHEN GRASS IS FOUR INCHES (4") TO SIX INCHES (6") HIGH. APPLY ONLY WHEN GRASS IS DRY.
5. UNLESS HYDROSEEDED, WORK IN LIME AND FERTILIZER TO A DEPTH OF FOUR (4") INCHES USING A DISK OR ANY SUITABLE EQUIPMENT.
6. TILLAGE SHOULD ACHIEVE A REASONABLY UNIFORM LOOSE SEEDBED. WORK ON CONTOUR IF SITE IS SLOPING.

ESTABLISHMENT:

1. APPLY SEED UNIFORMLY ACCORDING TO THE RATE INDICATED BY BROADCASTING, DRILLING, OR HYDRAULIC APPLICATION.
2. UNLESS HYDROSEEDED, COVER RYEGRASS SEEDS WITH NOT MORE THAN 1/4 INCH OF SOIL USING SUITABLE EQUIPMENT.
3. MULCH IMMEDIATELY AFTER SEEDING IF REQUIRED. (REFER TO TEMPORARY OR PERMANENT VEGETATIVE COVER REQUIREMENTS.) APPLY STRAW MULCH AND ANCHOR TO SLOPES GREATER THAN 3% OR WHERE CONCENTRATED FLOW WILL OCCUR.

PERMANENT VEGETATIVE COVER:

PERMANENT VEGETATIVE COVER SHALL BE ESTABLISHED AS VARIOUS SECTIONS OF THE PROJECT ARE COMPLETED IN ORDER TO STABILIZE THE SOIL, REDUCE DOWNSTREAM DAMAGE FROM SEDIMENT AND RUNOFF, AND TO ENHANCE THE AESTHETIC NATURE OF THE SITE. IT WILL BE APPLIED TO ALL CONSTRUCTION AREAS SUBJECT TO EROSION WHERE FINAL GRADING HAS BEEN COMPLETED AND A PERMANENT COVER IS NEEDED.

SITE PREPARATION:

1. INSTALL REQUIRED SURFACE WATER CONTROL MEASURES.
2. REMOVE LOOSE ROCK, STONE, AND CONSTRUCTION DEBRIS FROM AREA.
3. PERFORM ALL PLANTING OPERATIONS PARALLEL TO THE CONTOURS OF THE SLOPE.
4. APPLY TOPSOIL AS INDICATED ELSEWHERE HEREIN.
5. APPLY FERTILIZER ACCORDING TO SOIL TEST OR:
   - SPRING SEEDING: WORK DEEPLY IN SOIL, BEFORE SEEDING, 300 LBS. OF 10-10-10 FERTILIZER PER ACRE (7 LBS. PER 1,000 SQ. FT.); THEN SIX (6) TO EIGHT (8) WEEKS LATER, APPLY ON THE SURFACE AN ADDITIONAL 300LBS. OF 10-10-10 FERTILIZER PER ACRE. AFTER SEPTEMBER 1, TEMPORARY VEGETATIVE COVER SHALL BE APPLIED.
   - FALL SEEDING: WORK DEEPLY IN SOIL, BEFORE SEEDING, 600 LBS. OF 10-10-10 FERTILIZER PER ACRE (14 LBS. PER 1,000 SQ. FT.).

EROSION CHECKS:

GENERAL:

TEMPORARY PERVIOUS BARRIERS USING BALES OF STRAW, HELD IN PLACE WITH STAKES DRIVEN THROUGH THE BALES AND INTO THE GROUND OR GEOTEXTILE FABRIC FASTENED TO A FENCE POST AND BURIED INTO THE GROUND, SHALL BE INSTALLED AND MAINTAINED AS REQUIRED TO CHECK EROSION AND REDUCE SEDIMENTATION.

CONSTRUCTION:

BALES SHOULD BE PLACED IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.

EACH BALE SHALL BE EMBEDDED INTO THE SOIL A MINIMUM OF FOUR (4") INCHES.

BALES SHALL BE SECURELY ANCHORED IN PLACE BY WOOD STAKES OR REINFORCEMENT BARS DRIVEN THROUGH THE BALES AND INTO THE GROUND. THE FIRST STAKE IN EACH BALE SHALL BE ANGLED TOWARD THE PREVIOUSLY LAID BALE TO FORCE BALES TOGETHER.

GEOTEXTILE FABRIC SHALL BE SECURELY ANCHORED AT THE TOP OF A THREE FOOT (3') HIGH FENCE AND BURIED A MINIMUM OF FOUR INCHES (4") TO THE SOIL. SEAMS BETWEEN SECTIONS OF FILTER FABRIC SHALL OVERLAP MINIMUM OF TWO FEET (2').

INSTALLATION AND MAINTENANCE:

1. BALED STRAW EROSION BARRIERS SHALL BE INSTALLED AT ALL STORM SEWER INLETS.
2. BALED STRAW EROSION BARRIERS AND GEOTEXTILE FENCE SHALL BE INSTALLED AT THE LOCATION INDICATED ON THE PLAN AND IN ADDITIONAL AREAS AS MAY BE DEEMED APPROPRIATE DURING CONSTRUCTION.
3. ALL EROSION CHECKS SHALL BE MAINTAINED UNTIL ADJACENT AREAS ARE STABILIZED.
4. INSPECTION SHALL BE FREQUENT (AT MINIMUM BI-MONTHLY AND AFTER RAINFALL EVENTS GREATER THAN ONE HALF INCH) AND REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. EROSION CHECKS SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM WATER FLOW OR DRAINAGE.

TEMPORARY STABILIZATION FOR WINTER CONDITIONS:

ANY SIGNIFICANT AREAS OF EXPOSED SOIL WHICH HAVE BEEN DISTURBED AFTER OCTOBER 15TH SHALL BE TEMPORARILY STABILIZED BY ONE OF THE FOLLOWING METHODS UNTIL SUCH TIME THAT PERMANENT STABILIZATION MEASURES AND SEEDING CAN BE APPLIED, TYPICALLY AFTER MAY 15TH.

1. INSTALLATION OF AN ANCHORED EROSION CONTROL BLANKET. EROSION CONTROL BLANKETS SHOULD NOT BE INSTALLED ON SNOW OF GREATER THAN ONE INCH IN DEPTH OR ON FROZEN GROUND.
2. APPLICATION OF STRAW MULCH AT A RATE OF FOUR (4) TONS PER ACRE.
3. APPLICATION OF WOOD CHIP MULCH AT A MINIMUM DEPTH OF THREE INCHES (3"). WOOD CHIP MULCH SHOULD NOT BE USED ON SLOPES GREATER THAN 2:1 (H:V). ALL WOOD CHIP MULCH SHALL BE REMOVED PRIOR TO RESUMING SITE GRADING.

VEGETATIVE COVER SELECTION & MULCHING:

TEMPORARY VEGETATIVE COVER:

PERENNIAL RYEGRASS 3 LBS./1,000 SQ.FT. (IOLUIUM PERENNE)

PERMANENT VEGETATIVE COVER:

1. TURFGRASS MIX OR EQUAL:

   RECOMMENDED APPLICATION RATE: 1 POUND PER 1,750 SF
   SEED MIX SPECIES: CREEPING RED FESCUE (Festuca rubra var. rubra (endophyte enchanced)) - 15%, PERENNIAL RYEGRASS (Lolium multiflorum) - 15%, KENTUCKY BLUEGRASS (Poa pratensis 'KenBlue') - 35%, CHEWINGS FESCUE (Festuca rubra var. commutate 'Tiffany') - 15%.

   TEMPORARY MULCHING: STRAW AT 70-90 LBS./1,000 SQ.FT. (TEMPORARY VEGETATIVE AREAS) WOOD FIBER IN HYDROMULCH SLURRY 25-50 LBS./1,000 SQ. FT.

2. WITHIN 100-FOOT REGULATED UPLAND AREAS FROM WETLANDS:

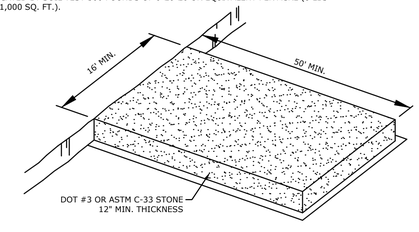
   RECOMMENDED APPLICATION RATE: 1 POUND PER 1,250 SF
   SEED MIX SPECIES: NEW ENGLAND EROSION CONTROL/RESTORATION MIX (FOR MOIST SITES) OR 1 POUND PER 1,750 SF NEW ENGLAND CONSERVATION/WILDLIFE MIX.

ESTABLISHMENT:

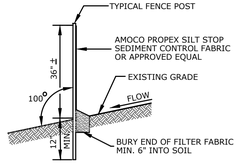
1. SMOOTH AND FIRM SEEDBED WITH CULTIPACKER OR OTHER SIMILAR EQUIPMENT PRIOR TO SEEDING (EXCEPT WHEN HYDROSEEDING).
2. SELECT ADAPTED SEED MIXTURE FOR THE SPECIFIC SITUATION. NOTE RATES AND THE SEEDING DATES (REFER TO TEMPORARY OR PERMANENT VEGETATIVE COVER REQUIREMENTS).
3. APPLY SEED UNIFORMLY ACCORDING TO RATE INDICATED, BY BROADCASTING, DRILLING, OR HYDRAULIC APPLICATION.
4. COVER GRASS AND LEGUME SEED WITH NOT MORE THAN 1/4 INCH OF SOIL WITH SUITABLE EQUIPMENT (EXCEPT WHEN HYDROSEEDING).
5. MULCH IMMEDIATELY AFTER SEEDING, IF REQUIRED, ACCORDING TO TEMPORARY MULCHING SPECIFICATIONS. (REFER TO TEMPORARY OR PERMANENT VEGETATIVE COVER REQUIREMENTS).
6. USE PROPER INOCULANT ON ALL LEGUME SEEDINGS, USE FOUR (4) TIMES NORMAL RATES WHEN HYDROSEEDING.
7. THE USE OF SOD IS AN ACCEPTABLE ALTERNATIVE WHERE THERE IS A HEAVY CONCENTRATION OF WATER AND IN CRITICAL AREAS WHERE IT IS IMPORTANT TO GET A QUICK VEGETATIVE COVER TO PREVENT EROSION.

MAINTENANCE:

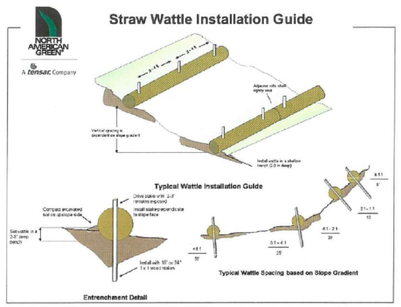
1. TEST FOR SOIL ACIDITY EVERY THREE (3) YEARS AND LIME AS REQUIRED.
2. ON SITES WHERE GRASSES PREDOMINATE, BROADCAST ANNUALLY 500 POUNDS OF 10-10-10 FERTILIZER PER ACRE (12 LBS. PER 1,000 SQ. FT.) OR AS NEEDED ACCORDING TO ANNUAL SOIL TESTS.
3. ON SITES WHERE LEGUMES PREDOMINATE, BROADCAST EVERY THREE (3)YEARS OR AS INDICATED BY SOIL TEST 300 POUNDS OF 0-20-20 OR EQUIVALENT PER ACRE (8 LBS PER 1,000 SQ. FT.).



TEMPORARY DUMPSTER PAD
NOT TO SCALE



GEOTEXTILE SILT FENCE (SF)
NOT TO SCALE



1. BEGIN AT THE LOCATION WHERE THE WATTLE IS TO BE INSTALLED BY EXCAVATING A 2-3" (5-7.5 CM) DEEP X 9" (22.9 CM) WIDE TRENCH ALONG THE CONTOUR OF THE SLOPE. EXCAVATED SOIL SHOULD BE PLACED UP-SLOPE FROM THE ANCHOR TRENCH.
2. PLACE THE WATTLE IN THE TRENCH SO THAT IT CONTOURS TO THE SOIL SURFACE. COMPACT SOIL FROM THE EXCAVATED TRENCH AGAINST THE WATTLE ON THE UPHILL SIDE. ADJACENT WATTLES SHOULD TIGHTLY ABUT.
3. SECURE THE WATTLE WITH 18-24" (45.7-61 CM) STAKES EVERY 3-4' (0.9 - 1.2 M) AND WITH A STAKE ON EACH END. STAKES SHOULD BE DRIVEN THROUGH THE MIDDLE OF THE WATTLE LEAVING AT LEAST 2-3" (5-7.5 CM) OF STAKE EXTENDING ABOVE THE WATTLE. STAKES SHOULD BE DRIVEN PERPENDICULAR TO SLOPE FACE.

North American Green Straw Wattles are a Best Management Practice (BMP) that offers an effective and economical alternative to silt fence and straw bales for sediment control and storm water runoff.

Guidelines are provided to assist in design, installation, and structure spacing. The guidelines may require modification due to variation in soil type, rainfall intensity or duration, and amount of runoff affecting the application site.

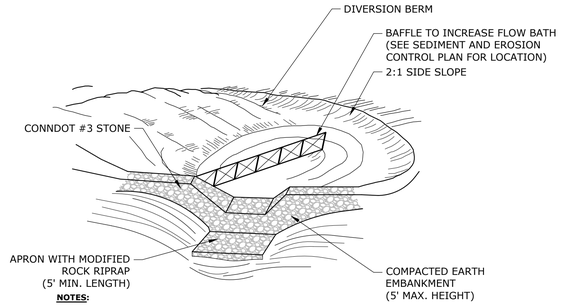
To maximize sediment containment with the Straw Wattle, place the initial structure at the toeslope of the slope if significant runoff is expected from above. If no runoff from above is expected, the initial Straw Wattle can be installed at the appropriate distance downhill from the toeslope of the slope. The final structure should be installed at or just beyond the bottomline of the slope. Wattles should be installed perpendicular to the primary direction of overland flow.

Straw Wattles are a temporary sediment control device and are not intended to replace listed erosion control products (RECPs) or hydraulic erosion control products (HECPs). If vegetation is desired for permanent erosion control, North American Green recommends that RECPs or HECPs be used to provide effective immediate erosion control until vegetation is established. Straw Wattles may be used in conjunction with blankets, mats, and mulches as supplemental sediment and runoff control for these applications. Like all sediment control devices, the effectiveness of the Straw Wattle is dependent on storage capacity.

For additional installation assistance, please contact North American Green's Technical Services Department at 1-800-772-2040

14649 Highway 41 North, Evansville, Indiana 47725
1-800-772-2040 www.nagreen.com Rev. 1/2008

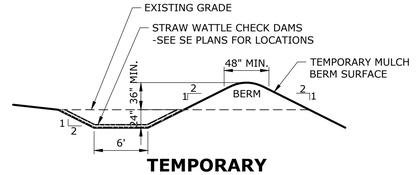
STRAW WATTLE (SW)
NOT TO SCALE



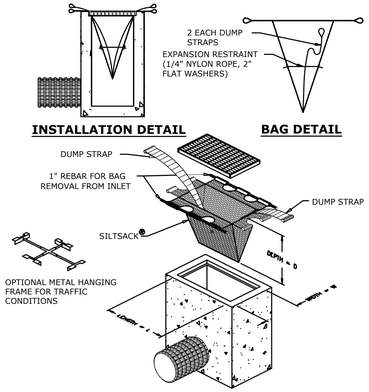
NOTES:

1. REFER TO SEDIMENT & EROSION CONTROL PLAN FOR APPROXIMATE DIMENSIONS AND REQUIRED VOLUME.
2. FOLLOWING GRADING OF THE SEDIMENT TRAP, A TEMPORARY RUBBERIZED IMPERVIOUS LINER SHALL BE INSTALLED ACROSS THE ENTIRE BOTTOM OF THE TRAP TO THE ELEVATION ALONG THE SIDE SLOPES THAT IS 1' ABOVE THE OUTLET ELEVATION. ALL OUTER EDGES OF THE LINER SHALL BE KEYED INTO SOIL TO A DEPTH OF 1' BELOW GRADE.
3. PRIOR TO DECOMMISSIONING THE SEDIMENT TRAP ALL ACCUMULATED SEDIMENT SHALL BE REMOVED AND THEN THE IMPERVIOUS LINER SHALL BE REMOVED.

TEMPORARY SEDIMENT TRAP
NOT TO SCALE



TEMPORARY DIVERSION BERM AND SWALE
NOT TO SCALE



INLET PROTECTION (IP)
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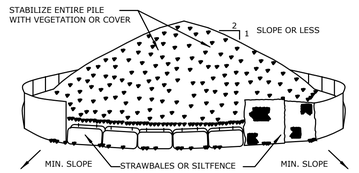

INSTALLATION NOTES

1. AREA CHOSEN FOR STOCKPILING OPERATIONS SHALL BE DRY AND STABLE.
2. MAXIMUM SLOPE OF STOCKPILE SHALL BE 1:2.
3. UPON COMPLETION OF SOIL STOCKPILING, EACH PILE SHALL BE SURROUNDED WITH EITHER SILT FENCING OR STRAWBALES, THEN STABILIZED WITH VEGETATION OR COVERED.
4. ALL STOCK PILES TO REMAIN IN PLACE FOR MORE THAN 30 CONSECUTIVE DAYS SHALL BE TEMPORARILY SEEDED.

STOCKPILE PROTECTION (STK)
NOT TO SCALE

EROSION CONTROL MAINTENANCE INTERVALS

EROSION CONTROL MEASURE	CONTROL OBJECTIVE	INSPECTION/MAINTENANCE	FAILURE INDICATORS	REMOVAL
TEMPORARY SEDIMENT TRAP (TST)	- DETAIN SEDIMENT-LADEN RUNOFF FROM SMALL DISTURBED AREAS LONG ENOUGH TO ALLOW A MAJORITY OF THE SEDIMENT TO SETTLE OUT.	INSPECT AT LEAST ONCE A WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL OF 0.5 INCHES OR MORE. STONE OUTLET SHOULD BE AT LEAST 0.5 FEET BELOW CREST OF EMBANKMENT. SEDIMENT MUST BE REMOVED WHEN ACCUMULATION REACHES ½ OF THE REQUIRED WET STORAGE.	- TURBID WATER - EXCESSIVE SEDIMENT ACCUMULATION - OVERTOPPING EVIDENCE	TST MAY BE REMOVED ONCE THE CONTRIBUTING DRAINAGE AREA IS PERMANENTLY STABILIZED.
SILT FENCE (SF) (RELATED: IP, STK)	- INTERCEPT, AND REDIRECT/DETAIN SMALL AMOUNTS OF SEDIMENT FROM SMALL DISTURBED AREAS. - DECREASE VELOCITY OF SHEET FLOW. - PROTECT SENSITIVE SLOPES OR SOILS FROM EXCESSIVE WATER FLOW.	INSPECT AT LEAST ONCE A WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL OF 0.5 INCHES OR MORE. ACCUMULATED SEDIMENT MUST BE REMOVED ONCE ITS DEPTH IS EQUAL TO ½ THE TRENCH HEIGHT. INSPECT FREQUENTLY DURING PUMPING OPERATIONS IF USED FOR DEWATERING OPERATIONS.	- PHYSICAL DAMAGE OR DECOMPOSITION - EVIDENCE OF OVERTOPPED OR UNDERCUT FENCE - EVIDENCE OF SIGNIFICANT FLOWS EVADING CAPTURE - REPETITIVE FAILURE	SILT FENCE MAY BE REMOVED AFTER UPHILL AND SENSITIVE AREAS HAVE BEEN PERMANENTLY STABILIZED.
STRAW WATTLE (SW)	- INTERCEPT, AND REDIRECT/DETAIN SMALL AMOUNTS OF SEDIMENT FROM SMALL DISTURBED AREAS. - DECREASE VELOCITY OF SHEET FLOW. - PROTECT SENSITIVE SLOPES OR SOILS FROM EXCESSIVE WATER FLOW.	INSPECT AT LEAST ONCE A WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL OF 0.5 INCHES OR MORE. ACCUMULATED SEDIMENT MUST BE REMOVED ONCE THE DEPTH OF SEDIMENT IS WITHIN 3" OF THE TOP OF THE BARRIER. INSPECT FREQUENTLY DURING PUMPING OPERATIONS IF USED FOR DEWATERING OPERATIONS.	- PHYSICAL DAMAGE OR DECOMPOSITION - EVIDENCE OF OVERTOPPED OR UNDERCUT FENCE - EVIDENCE OF SIGNIFICANT FLOWS EVADING CAPTURE - REPETITIVE FAILURE	STRAW WATTLE MAY BE REMOVED AFTER UPHILL AREAS HAVE BEEN PERMANENTLY STABILIZED.
TEMPORARY DIVERSION BERM/SWALE (TBS) OR TEMPORARY SWALE (TBS)	- MINIMIZE VELOCITY AND CONCENTRATION OF SHEET FLOW ACROSS CONSTRUCTION SITE TO A SEDIMENT TRAPPING FACILITY. - DIVERT WATER ORIGINATING FROM UNDISTURBED AREA AWAY FROM CONSTRUCTION.	WHEN LOCATED WITHIN CLOSE PROXIMITY TO ONGOING CONSTRUCTION ACTIVITIES, INSPECT AT THE END OF EACH WORK DAY AND IMMEDIATELY REPAIR DAMAGES. OTHERWISE INSPECT AT LEAST ONCE A WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL OF 0.5 INCHES OR MORE. REPAIR THE TEMPORARY MEASURE AND ANY OTHER ASSOCIATED MEASURES WITHIN 24 HOURS.	- PHYSICAL DAMAGE - EXCESSIVE SCOURING/EROSION - REPETITIVE FAILURE	TEMPORARY DIVERSIONS MAY BE REMOVED ONCE CONSTRUCTION HAS CEASED AND THE CONTRIBUTING DRAINAGE AREA HAS BEEN PERMANENTLY STABILIZED.
ANTI-TRACKING PAD (CE)	- REDUCE THE TRACKING OF SEDIMENT OFF-SITE ONTO PAVED SURFACES.	INSPECT AT THE END OF EACH WORK DAY AND IMMEDIATELY REPAIR DAMAGES. PERIODIC ADDITION OF STONE, OR LENGTHENING OF ENTRANCE MAY BE REQUIRED AS CONDITIONS DEMAND. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO PAVED SURFACES AS A RESULT OF INEFFICIENCY OF CONSTRUCTION ENTRANCE SHALL BE IMMEDIATELY REMOVED.	- SEDIMENT IN ROADWAY ADJACENT TO SITE	CONSTRUCTION ENTRANCE MAY BE REMOVED ONCE THE SITE HAS BEEN PERMANENTLY STABILIZED, AND ALL OTHER SECTIONS OF ROADWAY HAVE BEEN PERMANENTLY PAVED.
CATCH BASIN INLET PROTECTION (IP)	- PROHIBIT SILT IN CONSTRUCTION-RELATED RUNOFF FROM ENTERING STORM DRAINAGE SYSTEM.	INSPECT AFTER ANY RAIN EVENT. IF FILTER BAG INSIDE CATCH BASIN CONTAINS MORE THAN 6" OF SEDIMENT, REMOVE SEDIMENT FROM BAG. CHECK SURROUNDING SILT FENCE AND HAY BALES PER NOTED ABOVE.	- RIPPED BAG - FAILED HAY BALES / SILT FENCE - SIGNIFICANT SILT PRESENCE IN STORM DRAINAGE SYSTEM OUTFLOW.	INLET PROTECTION MAY BE REMOVED ONCE THE SITE HAS BEEN PERMANENTLY STABILIZED, AND ALL SECTIONS OF ROADWAY HAVE BEEN PERMANENTLY PAVED.
STOCKPILE PROTECTION (STK)	- RETAIN SOIL STOCKPILE IN LOCATIONS SPECIFIED, AND REDUCE WATER-TRANSPORT.	INSPECT SILT FENCE AT THE END OF EACH WORK DAY AND IMMEDIATELY REPAIR DAMAGES. PERIODIC REINFORCEMENT OF SILT FENCE, OR ADDITION OF HAY BALES MAY BE NECESSARY.	- EVIDENCE OF STOCK PILE DIMINISHING DUE TO RAIN EVENTS - FAILURE OF SILT FENCE	STOCKPILE PROTECTION MAY BE REMOVED ONCE THE STOCKPILE IS USED OR REMOVED.
DUST CONTROL	- TO PREVENT MOVEMENT OF DUST FROM EXPOSED SOIL SURFACES, WHICH MAY CAUSE BOTH OFF-SITE AND ON-SITE DAMAGE, BE A HEALTH HAZARD TO HUMANS, WILDLIFE, AND PLANT LIFE, OR CREATE A HAZARD BY REDUCING TRAFFIC VISIBILITY.	-USE MECHANICAL SWEEPING DAILY ON PAVED AREAS WHERE DUST AND FINE MATERIALS ACCUMULATE, IF HEAVILY TRAFFICKED AND SEDIMENT ACCUMULATES QUICKLY. MOISTEN UNPAVED TRAVELWAYS TO CONTROL DUST WHEN EVIDENCE OF AIRBORNE DUST.	-AIRBORNE DUST	-REPEAT APPLICATION OF DUST CONTROL MEASURES UNTIL ALL AREAS ARE PERMANENTLY STABILIZED, VEGETATED, AND PAVED, OR AS LONG AS THERE IS AIRBORNE DUST.

SLR
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SLRCONSULTING.COM

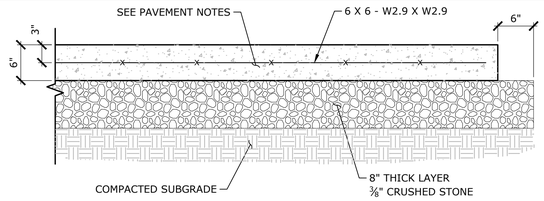
DESCRIPTION	DATE	BY
PUBLIC HEARING COMMENTS	12/8/2024	SM

SEDIMENT & EROSION CONTROL NOTES & DETAILS

WAKE ROBIN INN REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

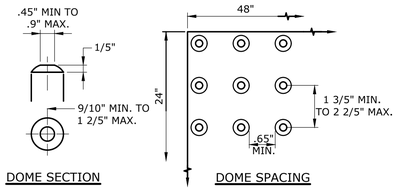
SM	SM	TR
DESIGNED	DRAWN	CHECKED

AS NOTED (SCALE)
JULY 29, 2024 (DATE)
22100.00001 (PROJECT NO.)
11 OF 23 (SHEET NO.)
SE-2 (SHEET NAME)



NOTE:
1. SEE UTILITY PAD PLAN VIEW FOR OVERALL DIMENSIONS.

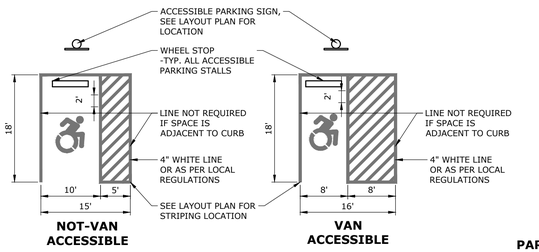
CONCRETE UTILITY PAD AT GENERATOR - TYPICAL SECTION
NOT TO SCALE



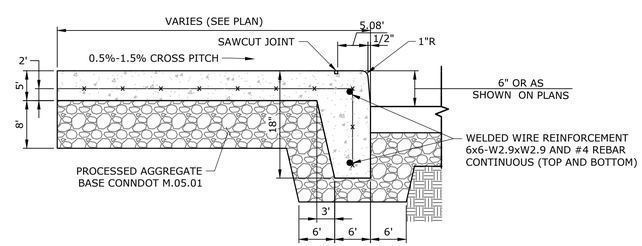
NOTES:

1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP OR ACCESSIBLE ROUTE SHOULD NOT EXCEED 20:1.
2. CARE SHALL BE TAKEN TO ASSURE UNIFORM GRADE ON THE RAMP, FREE OF SAGS AND ABRUPT GRADE CHANGES.
3. CONCRETE PER CONNDOT MATERIAL M.03.03, FORM 818
   - 3.1. PCC04462
   - 3.2. 4,000 PSI AT 28 DAYS
   - 3.3. CEMENT CONTENT OF 615LB MINIMUM
   - 3.4. AGGREGATE 3/4" MAX
   - 3.5. WATER TO CEMENT RATIO 0.42
4. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE ALONG ACCESSIBLE ROUTES SHALL BE STABLE, FIRM AND SLIP RESISTANT IN COMPLIANCE WITH ADAAG SECTION 4.5.
5. DIAGONAL SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES.
6. WHEN INSTALLING RAMPS WITHIN OR NEXT TO EXISTING WALKS, CUT ADJACENT WALKS TO THE NEAREST EXPANSION/CONTRACTION JOINT OR DUMMY JOINT. 12:1 MAY NOT BE ACHIEVABLE DUE TO SIDEWALK GRADE. IN RECOGNITION OF THIS, A MINIMUM LIMIT OF 15' FOR A PARALLEL RAMP SHALL BE USED.
7. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' UNLESS OTHERWISE NOTED.
8. TRANSITION TO FULL HEIGHT CURB. INSTALL STONE CURBING IF ADJACENT CURBING IS STONE. INSTALL CONCRETE CURBING IF ADJACENT CURBING IS CONCRETE OR BITUMINOUS.
9. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID. IN THE DIRECTION OF PEDESTRIAN TRAVEL.

DETECTABLE WARNING STRIP FOR ACCESSIBLE WALK
NOT TO SCALE

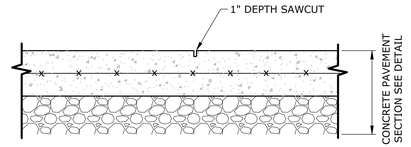


ACCESSIBLE PARKING SPACE LAYOUT & STRIPING
NOT TO SCALE



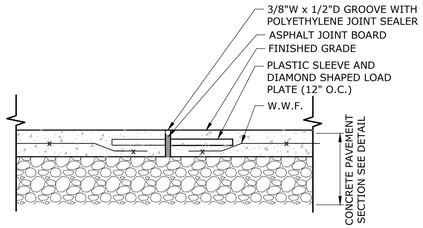
NOTES:
1. CONCRETE TO BE 4,000 PSI AT 28 DAYS. 1/2" EXPANSION JOINT AT INTERVALS NOT TO EXCEED 20'. EXPANSION JOINT TO RUN TO THE FACE OF CURB.
2. TO BE USED IN ALL LOCATIONS WHERE PROPOSED CONCRETE WALKS ABUT PROPOSED CONCRETE CURB.

INTEGRAL CONCRETE SIDEWALK CURB
NOT TO SCALE



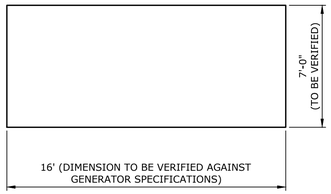
NOTES:
1. PROVIDE SAWCUTS AS SHOWN ON THE PLANS.

SCORE JOINT - SAWCUT
NOT TO SCALE



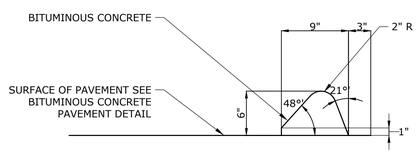
NOTES:
1. PROVIDE PREFORMED EXPANSION JOINT AT ALL CONSTRUCTION JOINT, AND OTHER LOCATIONS WHERE CONCRETE ABUTTS EXISTING CONCRETE.

CEMENT CONCRETE EXPANSION JOINT
NOT TO SCALE

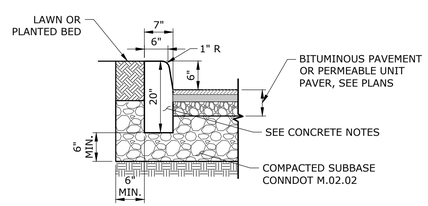


NOTES:
1. CONTRACTOR TO COORDINATE WITH EQUIPMENT MANUFACTURER FOR BOLT LOCATIONS, SLEEVES, AND ANY PENETRATIONS PRIOR TO POURING CONCRETE.
2. SEE CONCRETE UTILITY PAD-TYPICAL SECTION ON THIS SHEET.

CONCRETE UTILITY PAD AT GENERATOR - PLAN
NOT TO SCALE

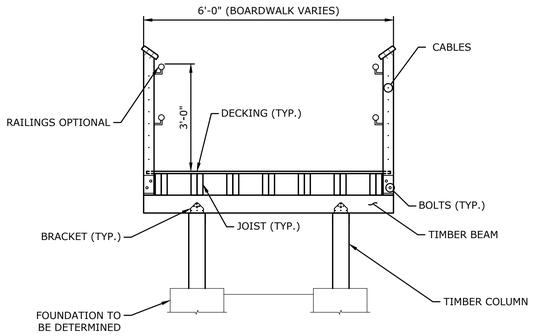


BITUMINOUS CONCRETE CURB
NOT TO SCALE

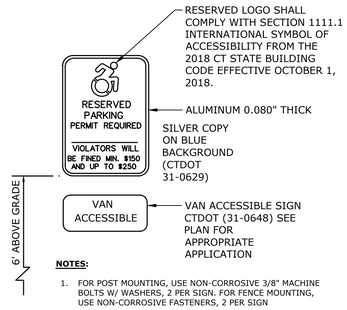


NOTE: DETAIL APPLIES WHERE CURB ABUTS LAWN OR PLANTED BED

CONCRETE OR GRANITE CURB
NOT TO SCALE

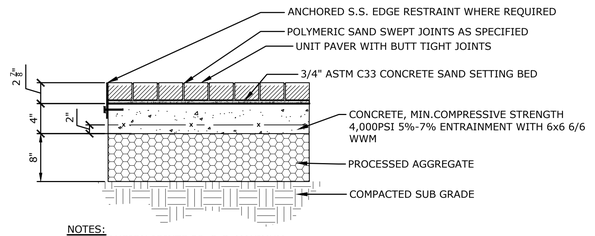


TYPICAL BOARDWALK/OVERLOOK SECTION
NTS



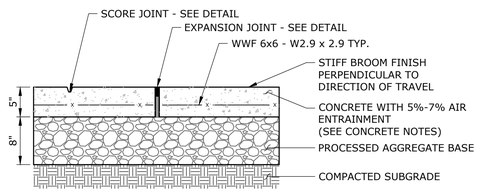
NOTES:
1. FOR POST MOUNTING, USE NON-CORROSIVE 3/8" MACHINE BOLTS W/ WASHERS, 2 PER SIGN. FOR FENCE MOUNTING, USE NON-CORROSIVE FASTENERS, 2 PER SIGN

RESERVED PARKING SIGN
NOT TO SCALE



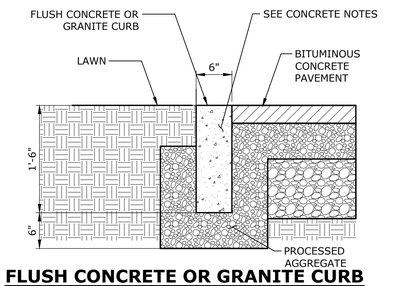
NOTES:
1. EXPANSION JOINTS 20' O.C. MAXIMUM.
2. CONCRETE BASE SHALL BE SCREEDED WITH A FLOAT FINISH AND PITCHED TO GRADE.

UNIT PAVERS ON CONCRETE BASE
NOT TO SCALE

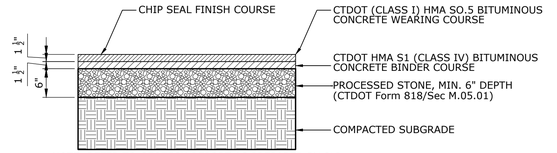


NOTES:
1. EXPANSION JOINTS 20' O.C. MAXIMUM CONSTRUCTION JOINTS 6' O.C. TYPICAL (OR AS SHOWN ON PLANS).
2. WWF SHALL BE INSTALLED UTILIZING CHAIR SUPPORTS.

CONCRETE PAVEMENT
NOT TO SCALE

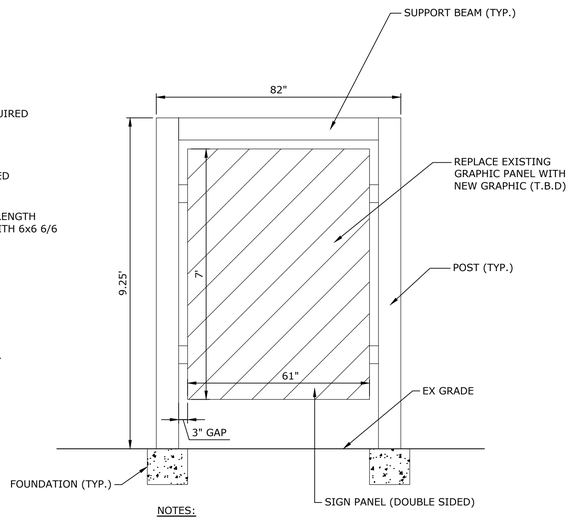


FLUSH CONCRETE OR GRANITE CURB
NOT TO SCALE



NOTE: REFER TO GEOTECH REPORT AS PREPARED BY SLR CONSULTING FOR ADDITIONAL INFORMATION REGARDING SUBGRADE AND PAVEMENT RECOMMENDATIONS

BITUMINOUS CONCRETE PAVEMENT
NOT TO SCALE



NOTES:
1. SIGN SHALL NOT EXCEED EXISTING SIGN DIMENSION
2. SIGN SUPPORT STRUCTURES MAY BE REPLACED WITH SIMILAR MATERIALS
3. FINAL SIGN GRAPHIC IS T.B.D.
4. SIGN WILL BE ILLUMINATED, BOTH SIDES

REPLACEMENT OF EXISTING WAKE ROBIN INN SIGNAGE
NOT TO SCALE

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

DESCRIPTION	DATE	BY
PAZ SUBMISSION	8/1/2024	SM
TOWN COMMENTS	11/6/2024	SM
PUBLIC HEARING COMMENTS	12/9/2024	TDR

SITE DETAILS

WAKE ROBIN INN REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	TR
DESIGNED	DRAWN	CHECKED

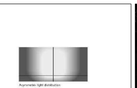
SCALE: AS NOTED

DATE: JULY 29, 2024

PROJECT NO.: 22100.00001

SHEET NO.: 12 OF 23

SHEET NAME: SD-1



Shielded bollards
with asymmetric light distribution

A series of bollards with shielded asymmetric light distribution.
The fully shielded design provides visual comfort while illuminating ground and walkways. Optical system is rotatable, so that the luminaire is be adjusted independent of anchor bolt orientation.
99/255 is optionally available with asymmetric wide beam (AWB) distribution for maximum luminaire spacing.

Die-cast and extruded aluminum - Clear safety glass with anti-reflective coating - Reflector made of pure anodized aluminum

LED color temperatures: 2700K, 3000K, 3500K, 4000K

EGSA luminaires offer a minimum service life of 60,000 hours, with available LED replacement modules guaranteed for up to 20 years after date of purchase. Further LED technical data including luminous flux, CRI, dimming and electrical characteristics are provided on the individual luminaire specification sheets, available at www.bega-us.com

All EGSA standard finishes are matte, textured powder coat with minimum 3 mil thickness. EGSA Unidure® finish, a fluoropolymer technology, provides superior fade protection in Black, Bronze, and Silver. EGSA standard White, as well as optionally available RAL and custom colors, are a polyester powder.

NRTL listed to North American standards - Suitable for wet locations - Protection class IP 65.

Bollard - asymmetric				
	HT	A	B	Wattage
99/04	11.8 in	7 in	21 in	70/87
99/08	11.8 in	7 in	39 in	70/87

263

Pole-top luminaires
with asymmetric light distribution

A series of pole-top luminaires with asymmetric light distribution. These luminaires are designed to illuminate roadways, squares, driveways and pedestrian areas.

Die-cast and extruded aluminum - Clear acrylic diffuser with optical texture - Reflector made of pure anodized aluminum

LED color temperatures: 2700K, 3000K, 3500K, 4000K

EGSA luminaires offer a minimum service life of 60,000 hours, with available LED replacement modules guaranteed for up to 20 years after date of purchase. Further LED technical data including luminous flux, CRI, dimming and electrical characteristics are provided on the individual luminaire specification sheets, available at www.bega-us.com

All EGSA standard finishes are matte, textured powder coat with minimum 3 mil thickness. EGSA Unidure® finish, a fluoropolymer technology, provides superior fade protection in Black, Bronze, and Silver. EGSA standard White, as well as optionally available RAL and custom colors, are a polyester powder.

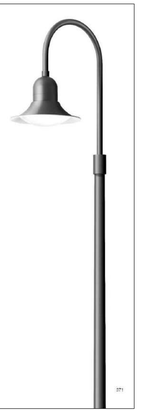
NRTL listed to North American standards - Suitable for wet locations - Protection class IP 65.

Available in bark roughness as a matte product.

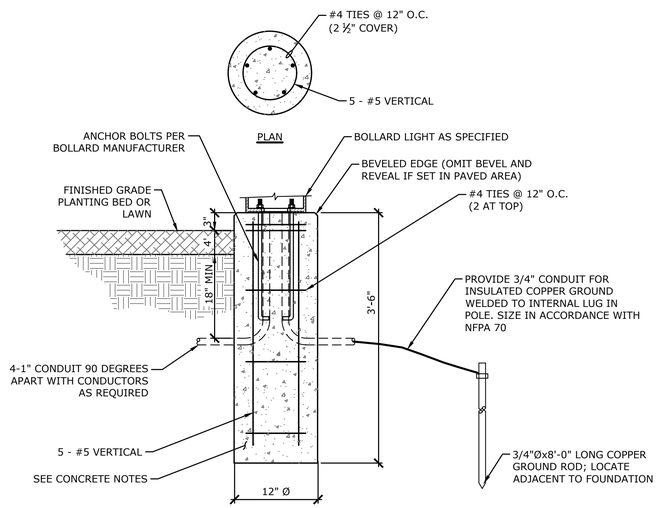
Pole-top luminaires				
	HT	A	B	C
77 915	15.4 in	13.7%	40%	51%
77 971	23.2 in	25%	40%	40%

Pole-top luminaires > see page 408-411

371



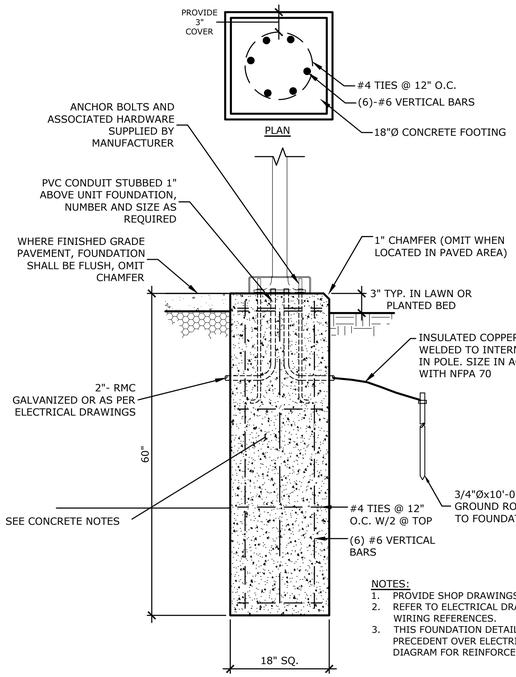
TYPICAL LIGHT BOLLARD
NOT TO SCALE



NOTES:
1. CONTRACTOR TO PROVIDE SHOP DRAWING FOR CONCRETE BASE AND BOLT PATTERN. BOLT PATTERN TO BE COORDINATED WITH BOLLARD LIGHT MANUFACTURER
2. REFER TO ELECTRICAL DRAWINGS FOR SPECIFIED FIXTURE
3. PROVIDE CONDUIT WITH CONDUCTORS

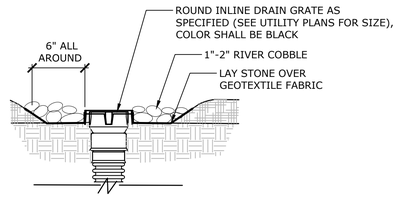
TYPICAL BOLLARD LIGHT FOUNDATION DETAIL
NOT TO SCALE

TYPICAL SITE LIGHT (12' HEIGHT)
NOT TO SCALE

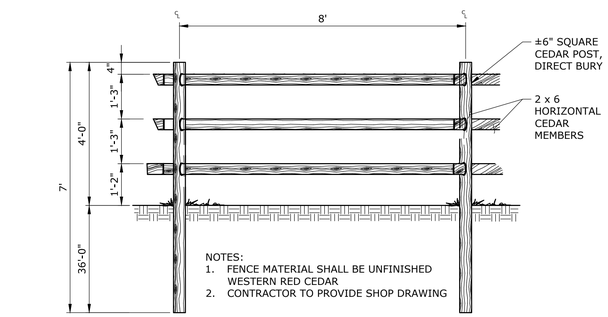


NOTES:
1. PROVIDE SHOP DRAWINGS FOR APPROVAL
2. REFER TO ELECTRICAL DRAWINGS FOR WIRING REFERENCES.
3. THIS FOUNDATION DETAIL SHALL TAKE PRECEDENT OVER ELECTRICAL FOOTING DIAGRAM FOR REINFORCEMENT

LIGHT POLE FOUNDATION DETAIL
NOT TO SCALE

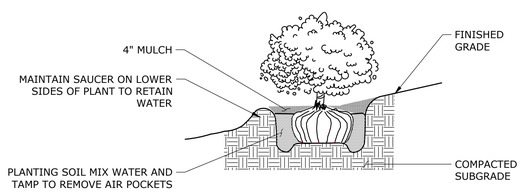


AREA DRAIN GRATE IN LANDSCAPED BED
NOT TO SCALE



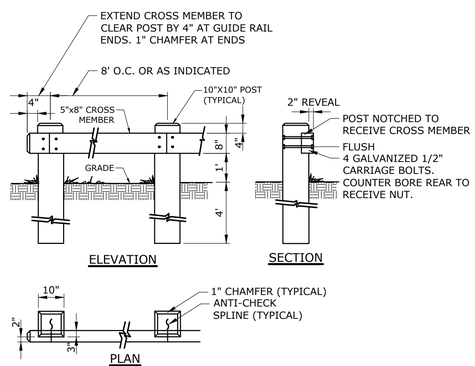
NOTES:
1. FENCE MATERIAL SHALL BE UNFINISHED WESTERN RED CEDAR
2. CONTRACTOR TO PROVIDE SHOP DRAWING

CEDAR 3-RAIL FENCE
NOT TO SCALE

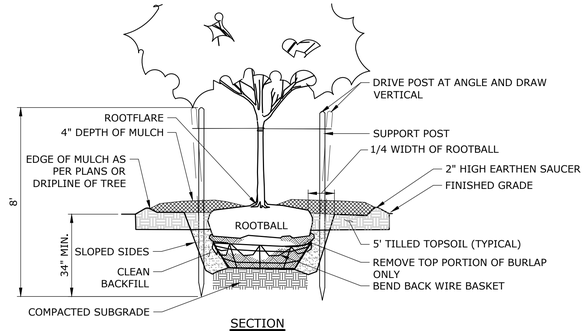


NOTES:
1. UNLESS OTHERWISE DIRECTED SHREDDED MULCH SHALL BE PLACED TO A LIMIT OF ONE FOOT BEYOND THE CENTER OF THE OUTERMOST SHRUBS IN SHRUB BED.

SHRUB PLANTING
NOT TO SCALE



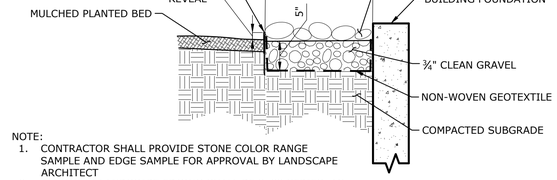
TIMBER GUIDE RAIL FACEMOUNT 10x10 POSTS
NOT TO SCALE



NOTE:

1. SUPPORT STAKES SHALL BE REMOVED BY THE CONTRACTOR ONE YEAR AFTER INSTALLATION.

TREE PLANTING
NOT TO SCALE

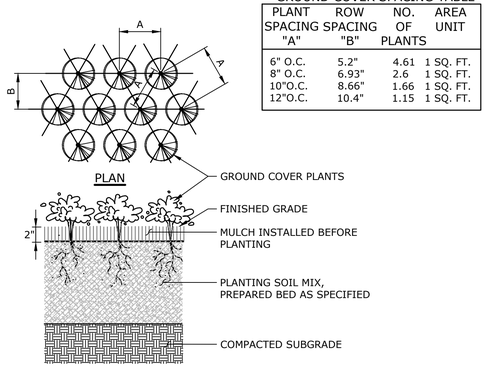


NOTE:
1. CONTRACTOR SHALL PROVIDE STONE COLOR RANGE SAMPLE AND EDGE SAMPLE FOR APPROVAL BY LANDSCAPE ARCHITECT
2. ALUMINUM LANDSCAPE EDGING SHALL BE 3500 SERIES, AS MANUFACTURED BY CURV-RITE, OR APPROVED EQUIVALENT

COBBLE MULCH DRIP EDGE
NOT TO SCALE

GROUND COVER SPACING TABLE

PLANT SPACING "A"	ROW SPACING "B"	NO. OF PLANTS	AREA UNIT
6" O.C.	5.2"	4.61	1 SQ. FT.
8" O.C.	6.93"	2.6	1 SQ. FT.
10"O.C.	8.66"	1.66	1 SQ. FT.
12"O.C.	10.4"	1.15	1 SQ. FT.



NOTES:

1. ALL GROUND COVER TO BE PLANTED IN TRIANGULAR PATTERN. SEE DETAIL PLAN AND GROUND COVER SPACING TABLE.

GROUND COVER/ PERENNIAL PLANTING
NOT TO SCALE

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

DESCRIPTION	DATE	BY

SITE DETAILS

WAKE ROBIN INN REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	TR
DESIGNED	DRAWN	CHECKED

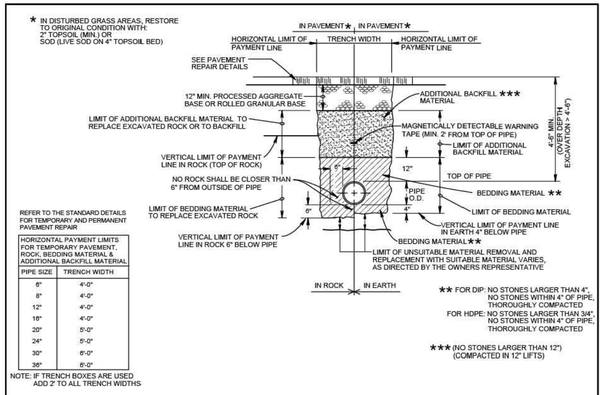
AS NOTED
SCALE

JULY 29, 2024
DATE

22100.00001
PROJECT NO.

13 OF 23
SHEET NO.

SD-2
SHEET NAME



REFER TO THE STANDARD DETAILS FOR TEMPORARY AND PERMANENT PAVEMENT REPAIR

HORIZONTAL PAYMENT LIMITS FOR TEMPORARY PAVEMENT, ROCK, BEDDING MATERIAL & ADDITIONAL BACKFILL MATERIAL	
PIPE SIZE	TRENCH WIDTH
6"	4'-0"
8"	4'-0"
12"	4'-0"
16"	4'-0"
20"	5'-0"
24"	5'-0"
30"	6'-0"
36"	6'-0"

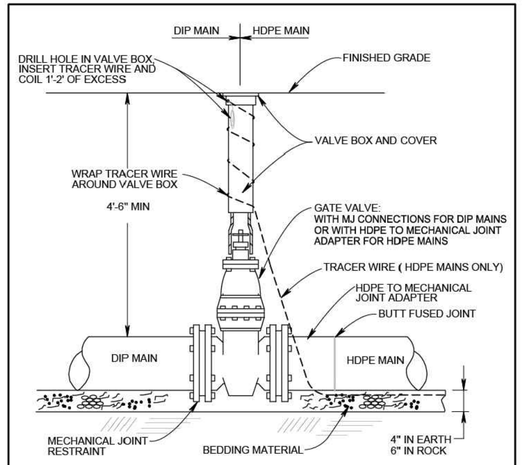
NOTE: IF TRENCH BOXES ARE USED ADD 2' TO ALL TRENCH WIDTHS

AQUARION Water Company — Stewards of the Environment

TYPICAL TRENCH DETAIL

NOT TO SCALE

SD-1



NOTE: ONLY INSTALLED ON MAINS 8" AND SMALLER

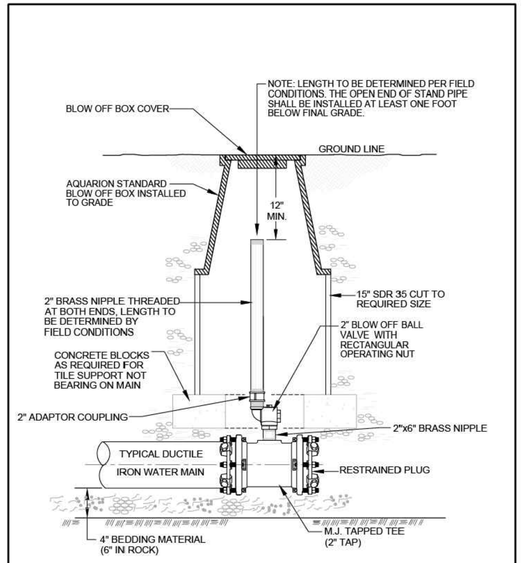
NOTE: FOR CONTINUOUS HDPE MAINS, INSTALL TRACER WIRE UNDER ALL VALVES AND FITTINGS. FOR TERMINATION POINTS, INSTALL TRACER WIRE AROUND VALVE BOXES AS SHOWN ABOVE.

AQUARION Water Company — Stewards of the Environment

GATE VALVE

NOT TO SCALE

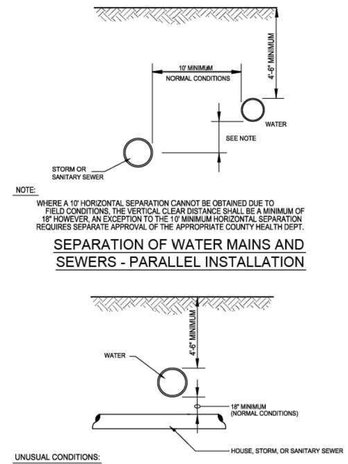
SD-2



AQUARION Water Company — Stewards of the Environment

BLOW-OFF AT DEAD END (DIP)

SD-6



NOTE:
WHERE A 10' HORIZONTAL SEPARATION CANNOT BE OBTAINED DUE TO FIELD CONDITIONS, THE VERTICAL CLEAR DISTANCE SHALL BE A MINIMUM OF 18" HOWEVER, AN EXCEPTION TO THE 10' MINIMUM HORIZONTAL SEPARATION REQUIRES SEPARATE APPROVAL OF THE APPROPRIATE COUNTY HEALTH DEPT.

SEPARATION OF WATER MAINS AND SEWERS - PARALLEL INSTALLATION

UNUSUAL CONDITIONS:
WATER MAINS PASSING UNDER SEWERS SHALL HAVE A VERTICAL CLEARANCE OF 18" MIN. BETWEEN THE SEWER AND WATER MAIN AND THE SEWER SHALL BE SUPPORTED AS DIRECTED BY THE OWNERS REPRESENTATIVE. TO THE EXTENT FEASIBLE, JOINTS IN THE WATER MAIN SHALL BE KEPT AS FAR AS POSSIBLE FROM THE SEWER. WHERE AN 18" SEPARATION CANNOT BE OBTAINED DUE TO FIELD CONDITIONS, SEPARATE APPROVAL OF THE HEALTH DEPARTMENT SHALL BE REQUIRED.

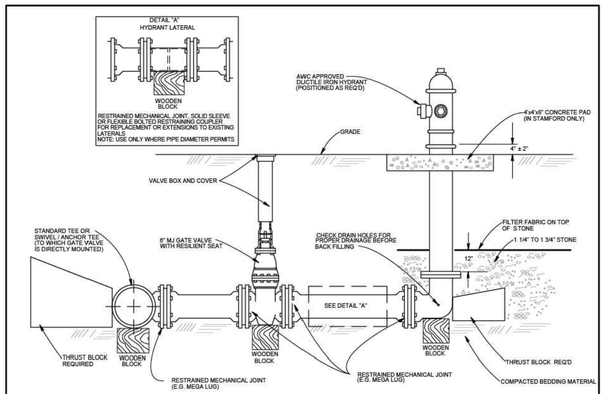
SEPARATION OF WATER MAINS AND SEWER CROSSINGS

AQUARION Water Company — Stewards of the Environment

PIPE INSTALLATION
SEPARATION OF WATER & SEWER CROSSINGS

NOT TO SCALE

SD-19

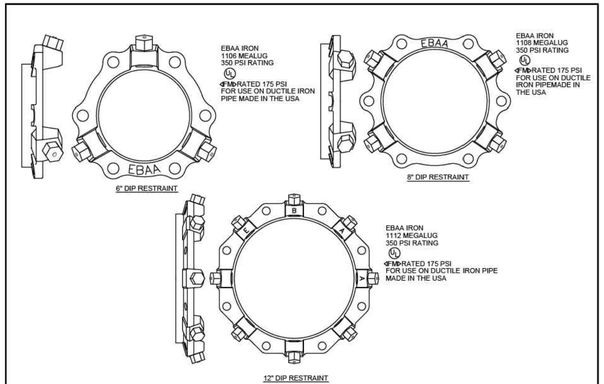


AQUARION Water Company — Stewards of the Environment

HYDRANT ASSEMBLY (DIP)

NOT TO SCALE

SD-8

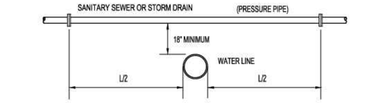


AQUARION Water Company — Stewards of the Environment

DUCTILE IRON PIPE RESTRAINTS

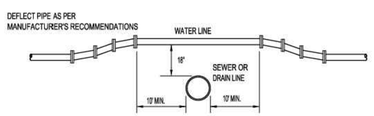
NOT TO SCALE

SD-10



NOTE:
WHERE WATER LINE PASSES BENEATH SEWER LINE OR STORM DRAIN, THE JOINTS OF THE PIPE SHALL BE A MIN. OF 10' FROM THE POINT OF CROSSING.

CROSSING OF WATER LINE UNDER SANITARY SEWER OR STORM DRAIN



NOTE:
FOR ANY CONDITIONS OTHER THAN SHOWN ABOVE THE FOLLOWING REQUIREMENTS SHALL BE MET. THE JOINTS OF THE SANITARY SEWER OR STORM DRAIN SHALL BE A MINIMUM OF 10' FROM THE POINT OF CROSSING.

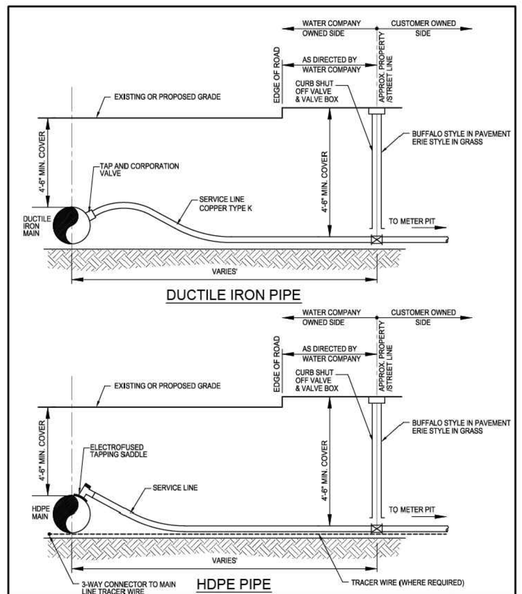
CROSSING OF WATER LINE OVER SANITARY SEWER OR STORM DRAIN

AQUARION Water Company — Stewards of the Environment

PIPE INSTALLATION
CROSSING SANITARY/STORM SEWER

NOT TO SCALE

SD-18



HDPE PIPE

AQUARION Water Company — Stewards of the Environment

PIPE INSTALLATION
WATER SERVICE LINE

NOT TO SCALE

SD-20

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

DESCRIPTION | DATE | BY

SITE DETAILS

WAKE ROBIN INN REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	TR
DESIGNED	DRAWN	CHECKED

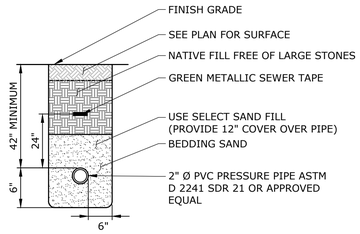
SCALE: AS NOTED

DATE: JULY 29, 2024

PROJECT NO.: 22100.00001

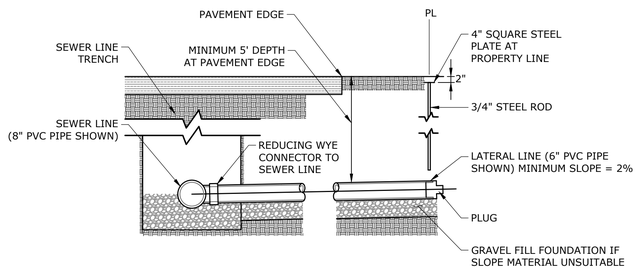
SHEET NO.: 14 OF 23

SHEET NAME: SD-3



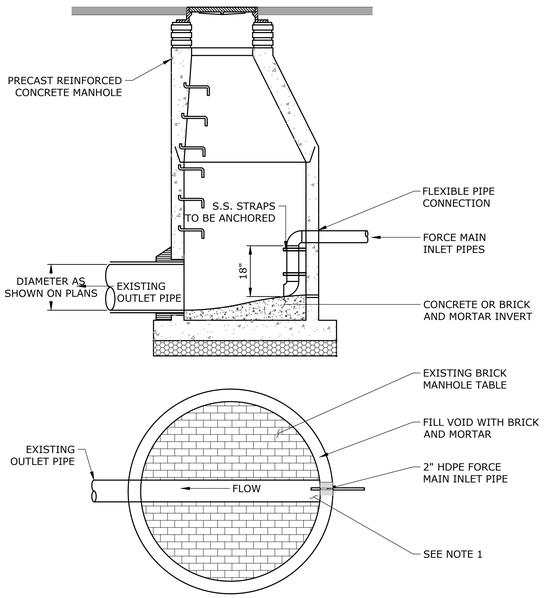
FORCE MAIN TRENCH

NOT TO SCALE



SERVICE LATERAL CONNECTION

NOT TO SCALE

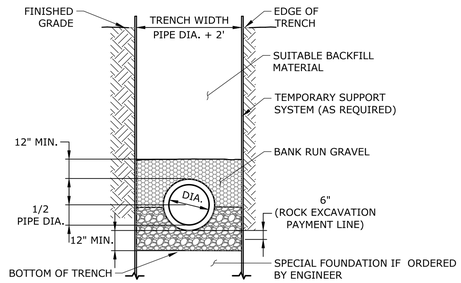


NOTES:

1. INVERT TO BE REBUILT UNDER SUPERVISION OF ENGINEER. A 24 HOUR ADVANCED NOTIFICATION IS REQUIRED FOR ALL INSPECTIONS.
2. NO OUTSIDE PIPE DROPS WILL BE ALLOWED.

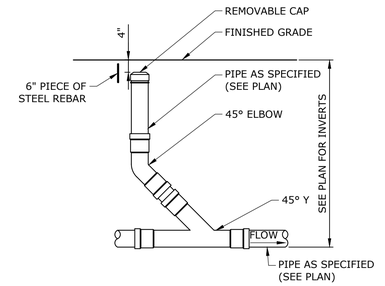
FORCE MAIN CONNECTION TO SEWER MANHOLE

NOT TO SCALE



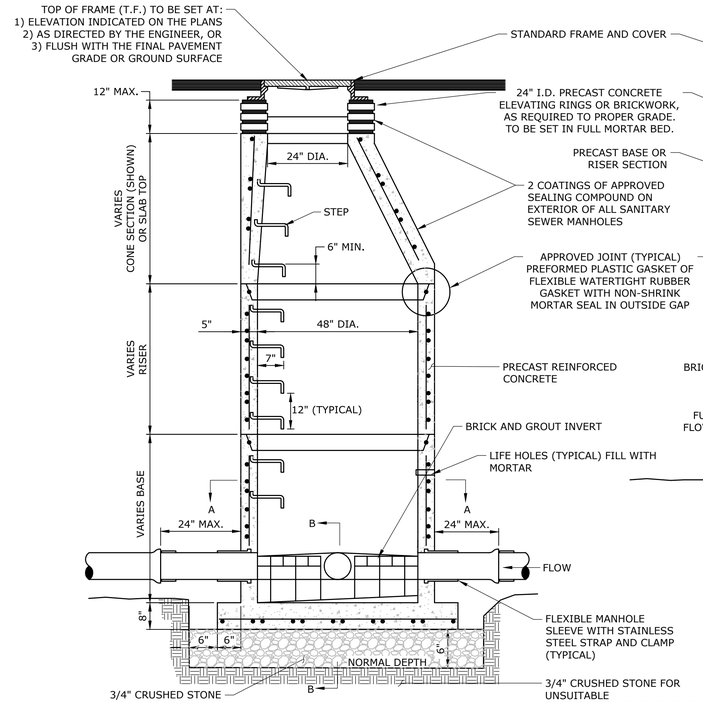
SANITARY SEWER TRENCH

NOT TO SCALE



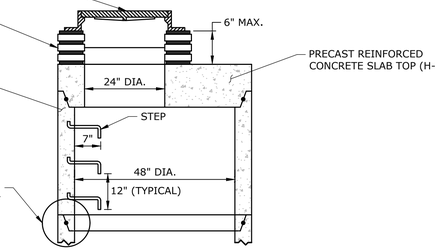
SANITARY CLEANOUT

NOT TO SCALE

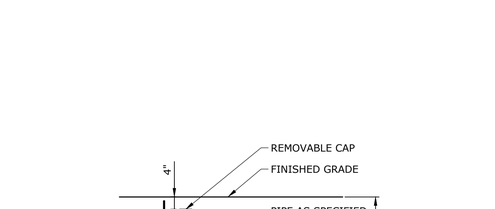


NOTES:

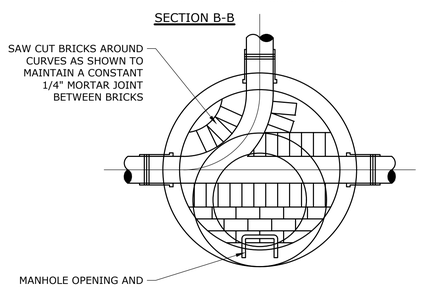
1. CONNECTIONS TO EXISTING SANITARY MANHOLES TO BE APPROVED BY WPCA AND CORE DRILLED WITH FLEX BOOT (FLEX-N-SEAL OR EQUAL)



MANHOLE WITH SLAB TOP



SECTION B-B



SECTION A-A

SANITARY MANHOLE

NOT TO SCALE

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

DESCRIPTION	DATE	BY

SITE DETAILS

WAKE ROBIN INN REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	TR
DESIGNED	DRAWN	CHECKED

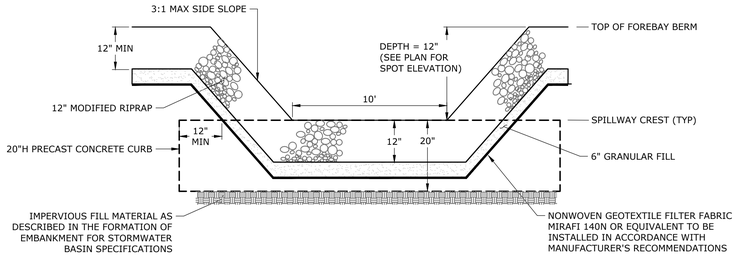
AS NOTED — SCALE

JULY 29, 2024 — DATE

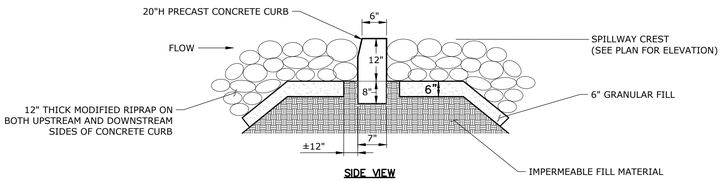
22100.00001 — PROJECT NO.

15 OF 23 — SHEET NO.

SD-4 — SHEET NAME



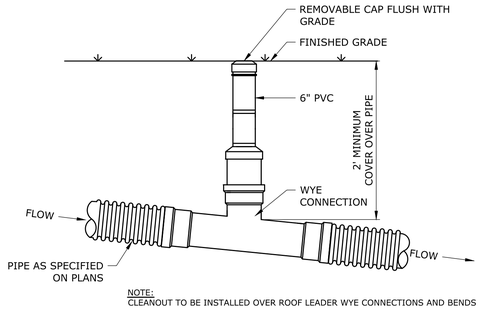
FRONT VIEW



SIDE VIEW

EMERGENCY RIPRAP SPILLWAY

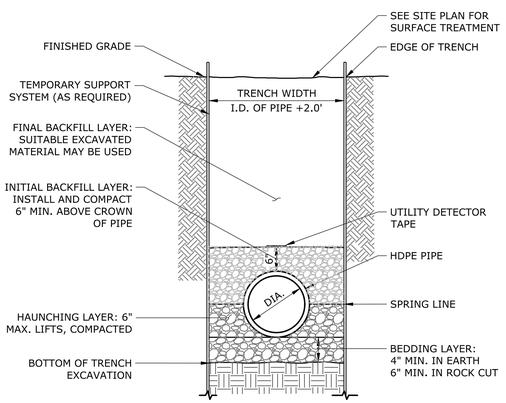
NOT TO SCALE



NOTE:
CLEANOUT TO BE INSTALLED OVER ROOF LEADER WYE CONNECTIONS AND BENDS

ROOF LEADER CLEANOUT

NOT TO SCALE

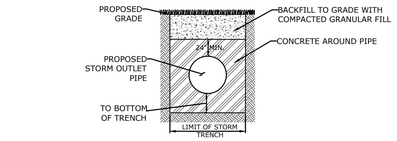


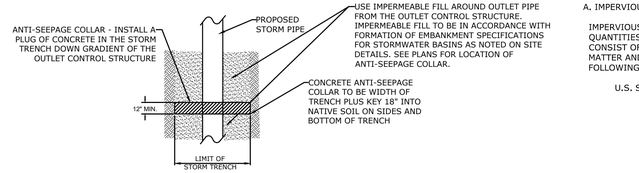
NOTES:

1. BACKFILL MATERIAL USED IN BEDDING, HAUNCHING, AND INITIAL BACKFILL LAYERS SHALL BE 3/4" CRUSHED STONE.
2. PAYMENT LIMIT FOR ROCK IN TRENCH TO BE PIPE DIAMETER + 3.0'

STORM DRAINAGE TRENCH

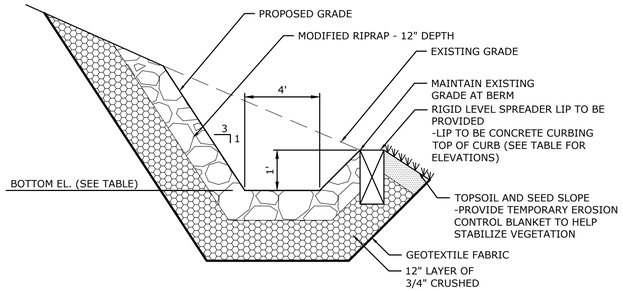
NOT TO SCALE





ANTI-SEEPAGE COLLAR

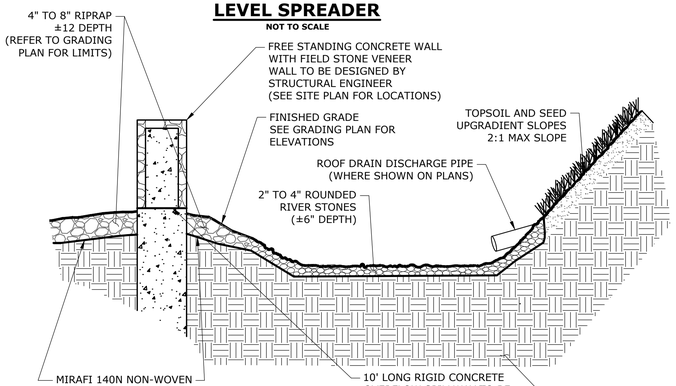
NOT TO SCALE



LEVEL SPREADER ID	BOTTOM EL. (FT)	TOP OF CURB EL. (FT)
220	800.0	801.0

LEVEL SPREADER

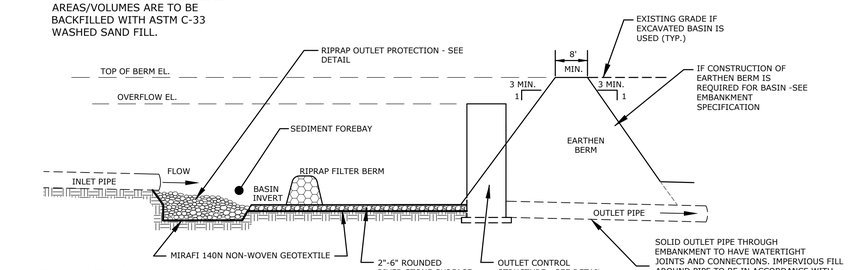
NOT TO SCALE



NOTE: CONTRACTOR TO PERFORM TEST PITS TO CONFIRM DEPTH OF LEDGE IS AT LEAST 3' BELOW THE BOTTOM FINISHED GRADE OF THE RAIN GARDEN. LEDGE SHALL BE EXCAVATED AS NECESSARY TO PROVIDE THE 3' VERTICAL SEPARATION. EXCAVATED LEDGE AREAS/VOLUMES ARE TO BE BACKFILLED WITH ASTM C-33 WASHED SAND FILL.

TYPICAL WATER QUALITY BASIN (RAIN GARDEN)

NOT TO SCALE



NOTE: CONTRACTOR TO PERFORM TEST PITS TO CONFIRM DEPTH OF LEDGE IS AT LEAST 3' BELOW THE BOTTOM FINISHED GRADE OF THE DETENTION BASIN. LEDGE SHALL BE EXCAVATED AS NECESSARY TO PROVIDE THE 3' VERTICAL SEPARATION. EXCAVATED LEDGE AREAS/VOLUMES ARE TO BE BACKFILLED WITH ASTM C-33 WASHED SAND FILL.

TYPICAL DETENTION BASIN

NOT TO SCALE

FORMATION OF EMBANKMENTS FOR STORMWATER BASINS

1. MATERIALS

ALL FILL MATERIALS SHALL BE OBTAINED FROM REQUIRED EXCAVATIONS OR DESIGNATED BORROW AREAS. FILL MATERIAL SHALL CONTAIN NO FROZEN MATERIAL, SOD, BRUSH, ROOTS, OR OTHER ORGANIC MATERIAL. EARTH EMBANKMENTS SHALL CONTAIN NO STONES OR ROCK PARTICLES OVER THREE INCHES IN DIAMETER.

THE MATERIAL USED IN THE CENTER PORTION OF THE EMBANKMENT SHALL BE THE MOST IMPERVIOUS MATERIAL OBTAINED FROM THE BORROW AREAS IF REQUIRED. THE MORE PERVIOUS MATERIALS SHALL BE USED IN THE OUTER PORTION OF THE EMBANKMENT AS SHOWN ON THE PLANS.

A. IMPERVIOUS FILL MATERIALS

IMPERVIOUS FILL SHALL BE A GLACIAL TILL, AND TO BE PROVIDED FROM AN OFFSITE SOURCE IN THE QUANTITIES REQUIRED FOR COMPLETION. FILL TO BE APPROVED BY THE ENGINEER. GLACIAL TILL SHALL CONSIST OF HARD AND DURABLE PARTICLES OR FRAGMENTS AND SHALL BE FREE FROM ORGANIC MATTER AND OTHER OBJECTIONABLE MATERIALS. GLACIAL TILL SHALL GENERALLY CONFORM TO THE FOLLOWING GRADATION LIMITS:

U.S. STANDARD SIEVE SIZE	PERCENTAGE PASSING BY WEIGHT
3 INCH	100
NO. 4	60-95
NO. 10	50-95
NO. 40	30-75
NO. 100	20-65
NO. 200	10-40

2. EMBANKMENT FOUNDATION PREPARATION

AREAS WHERE EMBANKMENTS ARE TO BE FORMED SHALL BE CLEARED AND GRUBBED OF ALL TOPSOIL AND OTHER ORGANIC MATERIALS TO A DEPTH OF AT LEAST 24 INCHES. UNLESS OTHERWISE SPECIFIED ON THE DRAWINGS, FOUNDATION AREAS SHALL BE SCARIFIED TO A DEPTH OF THREE INCHES PRIOR TO PLACEMENT OF FILL MATERIAL.

3. PLACEMENT

NO FILL SHALL BE PLACED UNTIL THE FOUNDATION PREPARATION AND EXCAVATIONS IN THE FOUNDATION HAVE BEEN COMPLETED. NO FILL SHALL BE PLACED ON A FROZEN SURFACE NOR SHALL FROZEN MATERIAL BE INCORPORATED.

A. EMBANKMENT

MATERIAL SHALL BE PLACED IN HORIZONTAL LAYERS. THE THICKNESS OF LAYERS SHALL BE SIX INCHES. DURING CONSTRUCTION, THE SURFACE OF THE FILL SHALL HAVE A CROWN OR CROSS-SLOPE OF NOT LESS THAN TWO PERCENT. EACH LAYER OR LIFT SHALL EXTEND OVER THE ENTIRE AREA OF THE FILL.

THE FILL SHALL BE FREE FROM LENSES, POCKETS, STREAKS, OR LAYERS OF MATERIAL DIFFERING SUBSTANTIALLY IN TEXTURE OR GRADATION FROM THE SURROUNDING MATERIAL. THE MORE PERVIOUS MATERIAL SHALL BE PLACED IN THE OUTSIDE PORTION OF THE EMBANKMENT OR AS INDICATED ON THE DRAWINGS. THE FINISHED FILL SHALL BE SHAPED AND GRADED TO THE LINES AND GRADE SHOWN ON THE DRAWINGS.

B. BACKFILL AT THE PIPE OUTLET

BACKFILL SHALL BE PLACED IN HORIZONTAL LAYERS NOT TO EXCEED THREE INCHES IN THICKNESS AND SHALL BE BROUGHT UP UNIFORMLY AROUND THE OUTLET PIPE AND FLARED END SECTION

4. MOISTURE CONTROL

THE MOISTURE CONTENT OF MATERIALS IN THE EMBANKMENT SHALL BE CONTROLLED TO MEET THE REQUIREMENTS OF SECTION 5, "COMPACTION OF EMBANKMENT." WHEN NECESSARY, MOISTURE SHALL BE ADDED BY USE OF APPROVED SPRINKLING EQUIPMENT. WATER SHALL BE ADDED UNIFORMLY AND EACH LAYER SHALL BE THOROUGHLY DISKED OR HARROWED TO PROVIDE ROPER MIXING. ANY LAYER FOUND TOO WET FOR PROPER COMPACTION SHALL BE ALLOWED TO DRY BEFORE ROLLING. PLACING OR ROLLING OF MATERIAL ON EARTH FILLS WILL NOT BE PERMITTED DURING OR IMMEDIATELY AFTER RAINFALLS WHICH INCREASE THE MOISTURE CONTENT BEYOND THE LIMIT OF SATISFACTORY COMPACTION. THE EARTH FILL SHALL BE BROUGHT UP UNIFORMLY AND ITS TOP SHALL BE KEPT GRADED AND SLOPED SO THAT A MINIMUM OF RAINWATER WILL BE RETAINED THEREON. COMPACTED EARTH FILL DAMAGED BY WASHING SHALL BE ACCEPTABLY REPLACED BY THE CONTRACTOR.

5. COMPACTION

A. EMBANKMENT

EMBANKMENT MATERIAL SHALL BE COMPACTED TO 95% OF THE STANDARD PROCTOR DENSITY AT NEAR OPTIMUM MOISTURE CONTENT AND BY THE COMPACTION EQUIPMENT SPECIFIED HEREIN. THE COMPACTION EQUIPMENT SHALL TRAVERSE THE ENTIRE SURFACE OF EACH LAYER OF FILL MATERIAL.

APPROVED TAMPING ROLLERS SHALL BE USED FOR COMPACTING ALL PARTS OF THE EMBANKMENTS WHICH THEY CAN EFFECTIVELY REACH, THE CONTRACTOR SHALL DEMONSTRATE THE EFFECTIVENESS OF THE ROLLER BY ACTUAL SOIL COMPACTION RESULTS OF THE SOIL TO BE USED IN THE EMBANKMENT WITH LABORATORY WORK PERFORMED BY AN APPROVED SOIL TESTING LABORATORY.

B. BACKFILL AT OUTLET CONDUIT

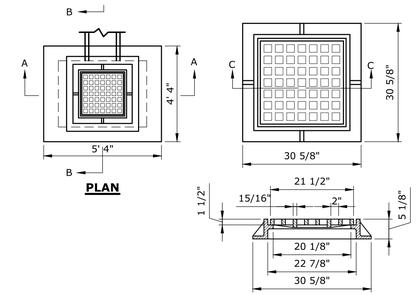
BACKFILL SHALL BE COMPACTED BY HAND TAMPING WITH MECHANICAL TAMPERS. HEAVY EQUIPMENT SHALL NOT BE OPERATED WITHIN TWO FEET OF ANY STRUCTURE. EQUIPMENT SHALL NOT BE ALLOWED TO OPERATE OVER THE OUTLET CONDUITS UNTIL THERE IS 24 INCHES OF FILL OVER THE PIPE CONDUITS.

6. FINISHING EMBANKMENTS

THE EMBANKMENTS SHALL BE CONSTRUCTED TO THE ELEVATIONS, LINES, GRADES AND CROSS-SECTIONS AS SHOWN ON THE DRAWINGS. THE EMBANKMENTS SHALL BE MAINTAINED IN A MANNER SATISFACTORY TO THE ENGINEER AND SURFACES SHALL BE COMPACT AND ACCURATELY GRADED BEFORE TOPSOIL IS PLACED ON THEM. THE CONTRACTOR SHALL CHECK THE EMBANKMENT SLOPES WITH STRING LINES TO INSURE THAT THEY CONFORM TO THE SLOPES GIVEN ON THE PLANS AND ARE UNIFORM FOR THE ENTIRE LENGTH OF THE SLOPE.

7. CONTROL OF WATER

THE PROJECT SITE IS SUBJECT TO HIGH WATER TABLE. THE CONTRACTOR SHALL USE TEMPORARY PIPES OR PUMPS TO ASSURE PLACEMENT OF SELECT FILL IN DRY CONDITIONS.



PLAN

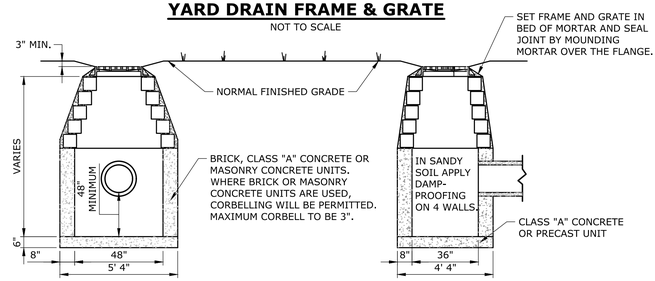
SECTION C-C

NOTES:

1. YARD DRAIN FRAMES & GRATES SHALL BE PATTERN #R-3404 AS MANUFACTURED BY THE "NEENAH FOUNDRY COMPANY" OF NEENAH, WISCONSIN, OR APPROVED EQUAL.

YARD DRAIN FRAME & GRATE

NOT TO SCALE



SECTION A-A **SECTION B-B**

NOTES:

1. WHERE PRECAST CONCRETE UNIT IS USED FOR SUMP, THE TOP OF THE UNIT SHALL BE AT LEAST 6" BELOW THE BOTTOM OF THE PIPE OUTLETTING FROM THE CATCH BASIN.

YARD DRAIN

NOT TO SCALE

DESCRIPTION	DATE	BY
PUBLIC HEARING COMMENTS	12/8/2024	TDR

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

SITE DETAILS

WAKE ROBIN INN
REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	SM	TR
DESIGNED	DRAWN	CHECKED

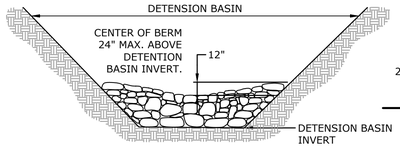
AS NOTED
SCALE

JULY 29, 2024
DATE

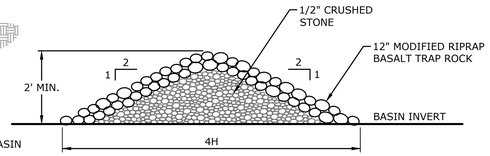
22100.00001
PROJECT NO.

16 OF 23
SHEET NO.

SD-5
SHEET NAME



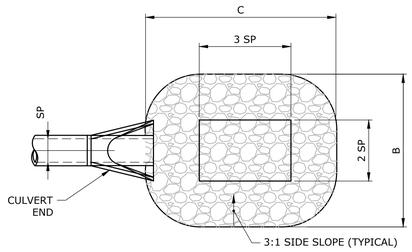
ELEVATION



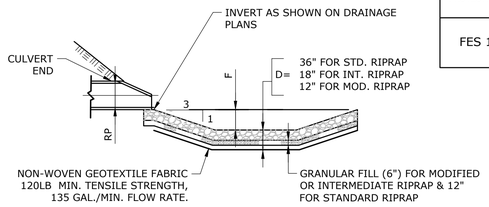
CROSS SECTION

RIPRAP FILTER BERM

NOT TO SCALE



PLAN

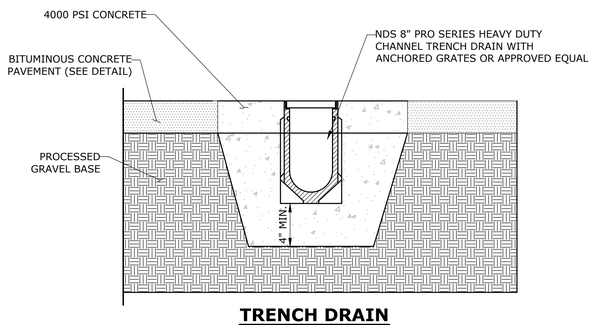


ELEVATION

OUTLET PROTECTION ID	TYPE	SP (FT)	RP (FT)	C (FT)	B (FT)	F (FT)	D (IN)
FES 12	MODIFIED TYPE 1	1.5	1.5	8.0	9.0	0.75	12

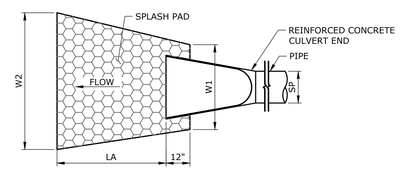
RIPRAP PREFORMED SCOUR HOLE

NOT TO SCALE

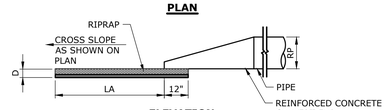


TRENCH DRAIN

NOT TO SCALE



PLAN



ELEVATION

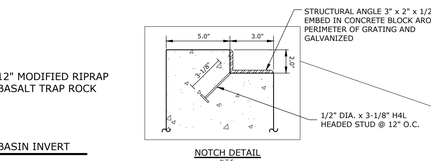
OUTLET PROTECTION ID	TYPE	SP (FT)	RP (FT)	LA (FT)	W1 (FT)	W2 (FT)	D (IN)
FES 23	MODIFIED TYPE B	0.67	0.67	10.0	2.0	6.0	12

FLARED END WITH RIP RAP SPLASH PAD

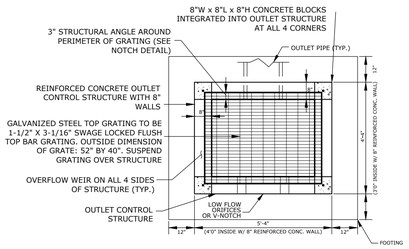
NOT TO SCALE

FALLING HEAD PERMEAMETER TEST RESULTS

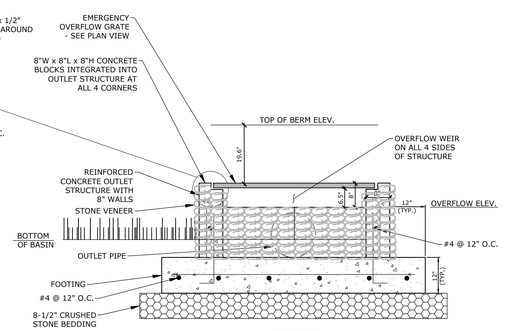
SAMPLE	K (IN/HR)	K (FT/DAY)	SAMPLE DEPTH (IN)
TP-1	10.63	21.26	32
TP-2	14.87	29.74	60
TP-3	26.42	52.84	32
TP-5	1.71	3.42	26
TP-7	13.95	27.90	18
TP-8	3.16	6.32	24
TP-9	0.34	0.68	22
TP-10A	0.12	0.24	26
TP-10B	0.65	1.30	32
TP-10C	0.26	0.52	31
TP-13	2.16	4.32	36
TP-14	2.78	5.56	36
TP-15	1.52	3.04	36
TP-16	2.20	4.40	34



NOTCH DETAIL
N.T.S.



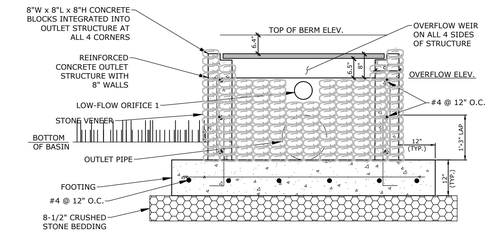
PLAN VIEW
N.T.S.



FRONT VIEW

DET 210

TOP OF BERM ELEVATION	818.2
OVERFLOW ELEVATION	815.9
100-YEAR WATER SURFACE ELEV.	817.2
OUTLET PIPE DIAMETER	15"
OUTLET PIPE INVERT	815.0
BASIN BOTTOM ELEVATION	815.0



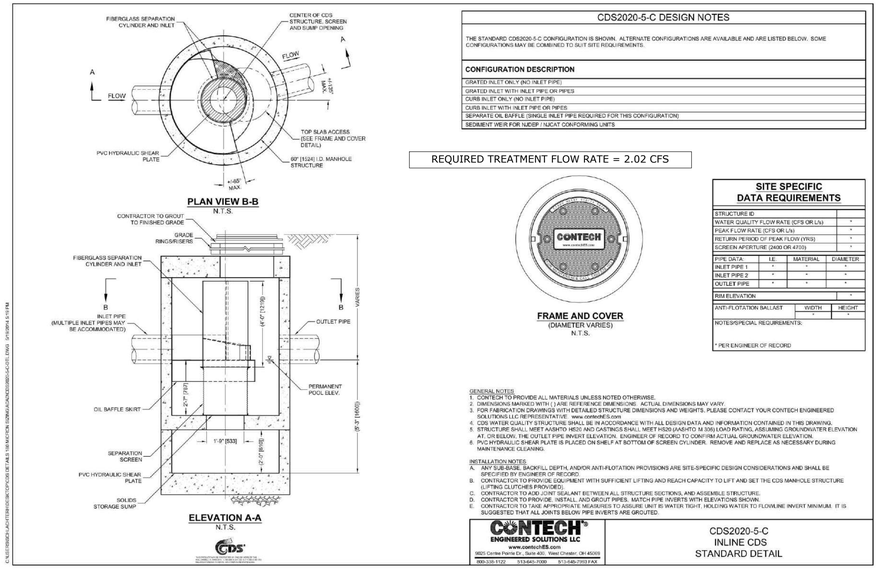
FRONT VIEW

DET 220

TOP OF BERM ELEVATION	804.0
OVERFLOW ELEVATION	802.8
100-YEAR WATER SURFACE ELEV.	803.0
LOW FLOW ORIFICE 1 DIAMETER	6"
LOW FLOW ORIFICE 1 INVERT	802.2
OUTLET PIPE DIAMETER	15"
OUTLET PIPE INVERT	800.5
BASIN BOTTOM ELEVATION	801.0

DETENTION BASIN OUTLET CONTROL STRUCTURES

SCALE: 1"=2'



CDS2020-5-C DESIGN NOTES

THE STANDARD CDS2020-5-C CONFIGURATION IS SHOWN. ALTERNATE CONFIGURATIONS ARE AVAILABLE AND ARE LISTED BELOW. SOME CONFIGURATIONS MAY BE COMBINED TO SUIT SITE REQUIREMENTS.

CONFIGURATION DESCRIPTION
GRATED INLET ONLY (NO INLET PIPE)
GRATED INLET WITH INLET PIPE OR PIPES
CURB INLET ONLY (NO INLET PIPE)
CURB INLET WITH INLET PIPE OR PIPES
SEPARATE OIL BAFFLE (SINGLE INLET PIPE REQUIRED FOR THIS CONFIGURATION)
SEDIMENT WEIR FOR IVDESP / IVDCAT CONFORMING UNITS

REQUIRED TREATMENT FLOW RATE = 2.02 CFS

FRAME AND COVER
(DIAMETER VARIES)
N.T.S.

SITE SPECIFIC DATA REQUIREMENTS			
STRUCTURE ID			
WATER QUALITY FLOW RATE (CFS OR L/s)			*
PEAK FLOW RATE (CFS OR L/s)			*
RETURN PERIOD OF PEAK FLOW (YRS)			*
SCREEN APERTURE (2400 OR 4700)			*
PIPE DATA:	I.E.	MATERIAL	DIAMETER
INLET PIPE 1	*	*	*
INLET PIPE 2	*	*	*
OUTLET PIPE	*	*	*
RIM ELEVATION			
ANTI-FLOTATION BALLAST	WIDTH		HEIGHT
	*		*
NOTES/SPECIAL REQUIREMENTS:			
* PER ENGINEER OF RECORD			

GENERAL NOTES
1. CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
2. DIMENSIONS MARKED WITH [ ] ARE REFERENCE DIMENSIONS. ACTUAL DIMENSIONS MAY VARY.
3. FOR FABRICATION DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHTS, PLEASE CONTACT YOUR CONTECH ENGINEERED SOLUTIONS LLC REPRESENTATIVE: www.contech-cpi.com
4. CDS WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
5. STRUCTURE SHALL MEET AASHTO H20 AND CASTINGS SHALL MEET H20 (AASHTO M 306) LOAD RATING, ASSUMING GROUNDWATER ELEVATION AT, OR BELOW, THE OUTLET PIPE INVERT ELEVATION. ENGINEER OF RECORD TO CONFIRM ACTUAL GROUNDWATER ELEVATION.
6. PVC HYDRAULIC SHEAR PLATE IS PLACED ON SHELF AT BOTTOM OF SCREEN CYLINDER. REMOVE AND REPLACE AS NECESSARY DURING MAINTENANCE CLEANING.

INSTALLATION NOTES:
A. ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
B. CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE CDS MANHOLE STRUCTURE (LIFTING CLUTCHES PROVIDED).
C. CONTRACTOR TO ADD JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS, AND ASSEMBLE STRUCTURE.
D. CONTRACTOR TO PROVIDE, INSTALL, AND GROUT PIPES. MATCH PIPE INVERTS WITH ELEVATIONS SHOWN.
E. CONTRACTOR TO TAKE APPROPRIATE MEASURES TO ASSURE UNIT IS WATER TIGHT, HOLDING WATER TO FLOWLINE INVERT MINIMUM. IT IS SUGGESTED THAT ALL JOINTS BELOW PIPE INVERTS ARE GROUTED.

CONTECH ENGINEERED SOLUTIONS LLC
www.contech-cpi.com
9025 Centre Pointe Dr., Suite 400, West Chester, OH 45069
800-338-1122 513-645-7000 513-645-7993 FAX

CDS2020-5-C
INLINE CDS
STANDARD DETAIL

CONTECH CDS 2020-5-C

NOT TO SCALE

TEST PIT LOGS

OBSERVED & DOCUMENTED BY: TODD RITCHIE, P.E.
DATE: JUNE 21, 2024
MACHINE EXCAVATED TEST PITS

TP-1

0-5" TOPSOIL
5-70" BROWN SILT LOAM
70-115" ORANGE/BROWN FINE SAND
NO LEDGE
NO GROUNDWATER
REDOX-70"
TUBE SAMPLE-32"

TP-2
0-42" TOPSOIL/FILL
42-44" ORIG. TOPSOIL
44-80" BROWN SILT LOAM
LEDGE-80"
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-60"

TP-3
0-3" GRAVEL
3-24" SAND AND GRAVEL
24-100" GREY SAND/SILT (COMPACT)
NO LEDGE
GROUNDWATER-64"
REDOX-24"
TUBE SAMPLE-32"

TP-4

0-5" TOPSOIL
5-36" BROWN SANDY LOAM
5-72" GREY SAND/SILT/GRAVEL (COMPACT)
LEDGE-72"
GROUNDWATER-36"
REDOX-36"

TP-5
0-2" ORGANIC/LEAF LITTER
2-56" BROWN SANDY LOAM
LEDGE-80"
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-26"

TP-6
0-2" ORGANIC/LEAF LITTER
2-12" BROWN SANDY LOAM
LEDGE-12"
NO GROUNDWATER
NO REDOX

TP-7
0-2" ORGANIC/LEAF LITTER
2-36" BROWN SANDY LOAM
36-52" TAN SAND
LEDGE-52"
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-18"

OBSERVED & DOCUMENTED BY:
SEAN MCALLEN, EIT
DATE: SEPTEMBER 12, 2024
HAND EXCAVATED TEST PITS

TP-8
0-2" ORGANIC/LEAF LITTER
2-36" BROWN SANDY LOAM
NO REFUSAL
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-24"

TP-9
0-4" TOPSOIL
4-22" SAND AND GRAVEL
COMPACT AT 22"
NO GROUNDWATER
POSSIBLE REDOX AT 22"
TUBE SAMPLE-22"

TP-10
0-2" ORGANIC/LEAF LITTER
2-32" BROWN SANDY LOAM
NO REFUSAL
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-32"

OBSERVED & DOCUMENTED BY:
SEAN MCALLEN, EIT
DATE: NOVEMBER 27, 2024
HAND EXCAVATED TEST PITS

TP-11
0-2" ORGANIC/LEAF LITTER
2-24" BROWN SANDY LOAM
REFUSAL AT 24"
NO GROUNDWATER
NO REDOX

TP-12
0-2" ORGANIC/LEAF LITTER
2-24" BROWN SANDY LOAM
REFUSAL AT 24"
NO GROUNDWATER
NO REDOX

TP-13
0-4" TOPSOIL
4-36" SAND AND GRAVEL
COMPACT AT 36"
NO GROUNDWATER
POSSIBLE REDOX AT 36"
TUBE SAMPLE-36"

TP-14
0-2" ORGANIC/LEAF LITTER
2-36" BROWN SANDY LOAM
NO REFUSAL
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-36"

TP-15
0-2" ORGANIC/LEAF LITTER
2-36" BROWN SANDY LOAM
NO REFUSAL
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-36"

TP-16
0-2" ORGANIC/LEAF LITTER
2-36" BROWN SANDY LOAM
NO REFUSAL
NO GROUNDWATER
NO REDOX
TUBE SAMPLE-34"

SLR
99 REALTY DRIVE
CHELMSFORD, MA 01410
(978) 251-7113
SLRCONSULTING.COM

DESCRIPTION	DATE	BY
PAZ SUBMISSION	8/1/2024	SM
PEER REVIEW COMMENTS	9/13/2024	MCB
TOWN COMMENTS	11/6/2024	MCB
PUBLIC HEARING COMMENTS	12/9/2024	TDR

SITE DETAILS

WAKE ROBIN INN
REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

MB	MB	TR
DESIGNED	DRAWN	CHECKED

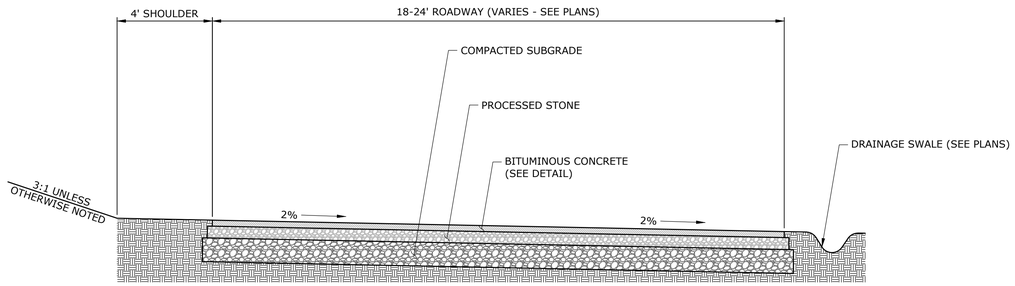
AS NOTED
SCALE

JULY 29, 2024
DATE

22100.00001
PROJECT NO.

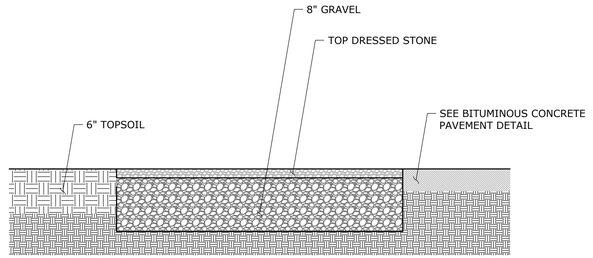
17 OF 23
SHEET NO.

SD-6
SHEET NAME



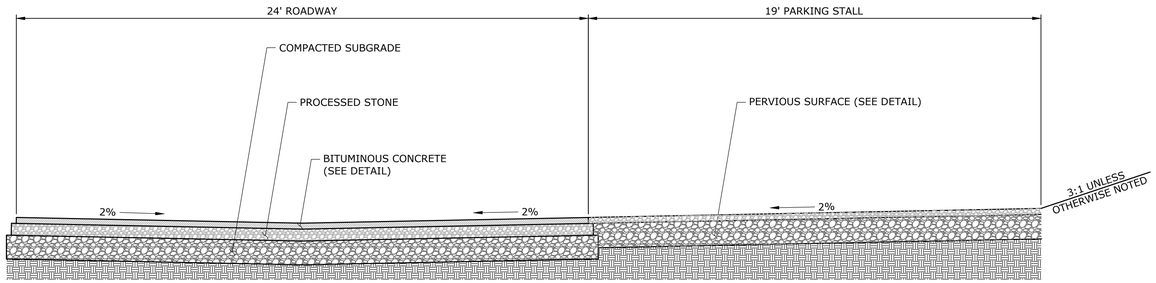
ROADWAY SECTION (SWALE)

SCALE 1:3



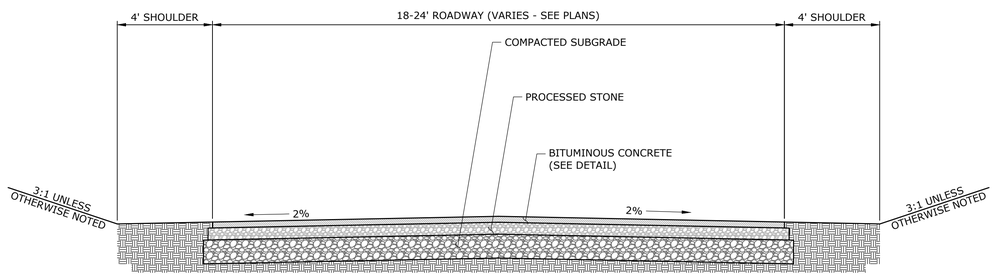
GRAVEL SURFACE

NOT TO SCALE



ROADWAY SECTION (REVERSE CROWN)

SCALE 1:3



ROADWAY SECTION (CROWN)

SCALE 1:3

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

DESCRIPTION	DATE	BY
PUBLIC HEARING COMMENTS	12/8/2024	TDR

SITE DETAILS

WAKE ROBIN INN
REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	DR	TR
DESIGNED	DRAWN	CHECKED

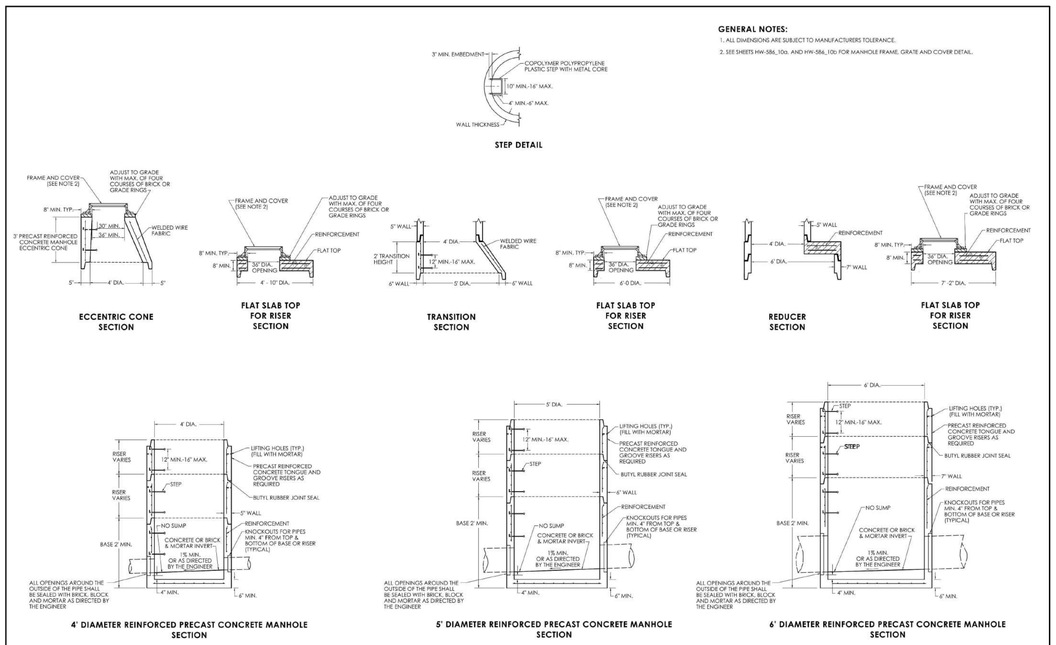
AS NOTED
SCALE

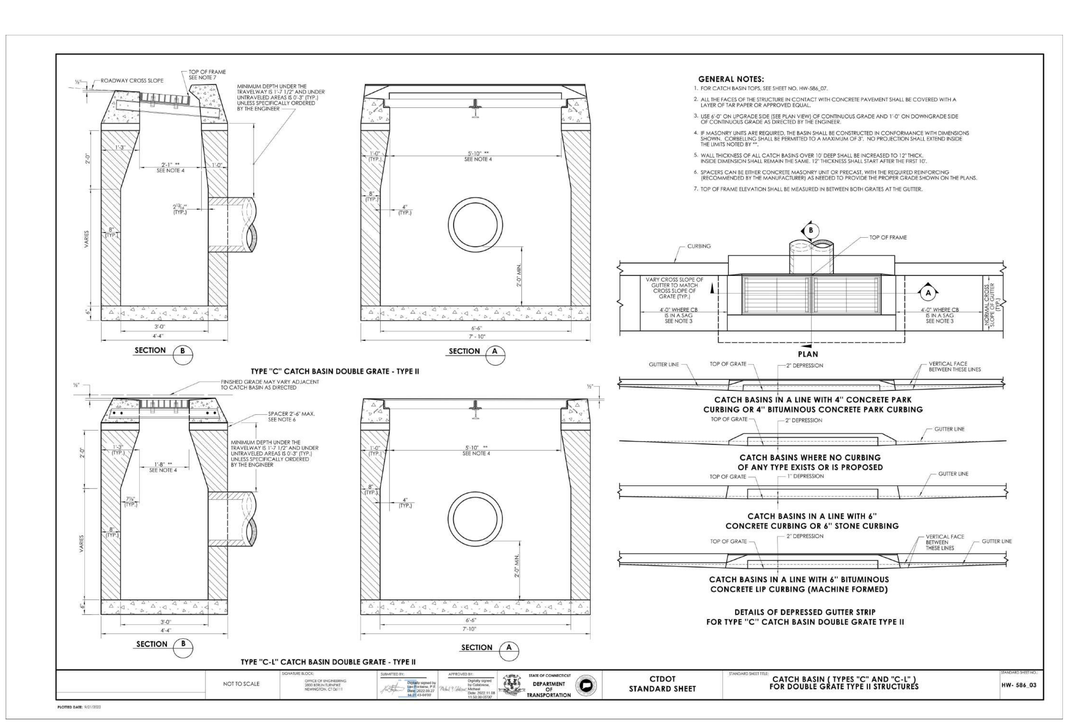
SEPTEMBER 6, 2024
DATE

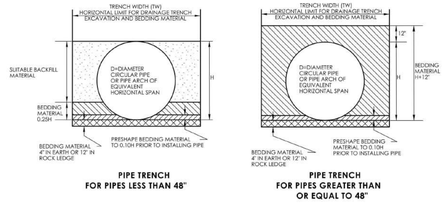
22100.00001
PROJECT NO.

18 OF 23
SHEET NO.

SD-7
SHEET NAME



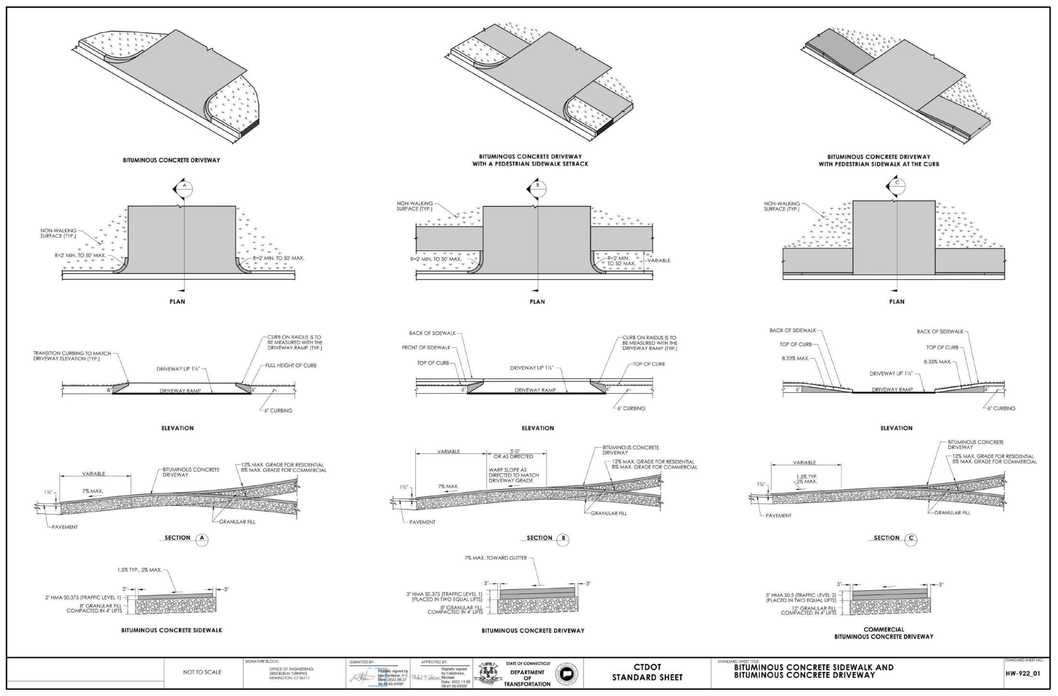




TRENCH WIDTH (TW) CHART

PIPE, PIPE-ARCH, OR DRAINAGE STRUCTURE	TRENCH WIDTH
PIPE OR PIPE-ARCH WITH NOMINAL INSIDE HORIZONTAL SPAN LESS THAN 30"	2' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
PIPE OR PIPE-ARCH WITH NOMINAL INSIDE HORIZONTAL SPAN GREATER THAN OR EQUAL TO 30"	3' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
PIPE OR PIPE-ARCH FABRICATED FROM STRUCTURAL PLATES	4' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
DRAINAGE STRUCTURES	2' BEYOND ALL EXTERIOR OF FOUNDATION WALLS

NOT TO SCALE
SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111
SUBMITTED BY:
APPROVED BY:
Digitally signed by Gabrielle Bernier Date: 2022.11.08 14:58:26 -05'00'
STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION
CTDOT STANDARD SHEET
STANDARD SHEET TITLE: DRAINAGE TRENCH EXCAVATION
STANDARD SHEET NO.: HW-286_01



NOT TO SCALE
SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111
SUBMITTED BY:
APPROVED BY:
Digitally signed by Gabrielle Bernier Date: 2022.11.08 15:01:36 -05'00'
STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION
CTDOT STANDARD SHEET
STANDARD SHEET TITLE: BITUMINOUS CONCRETE SIDEWALK AND BITUMINOUS CONCRETE DRIVEWAY
STANDARD SHEET NO.: HW-922_01

SLR
99 REALTY DRIVE
CHELMSFORD, MA 01824
203.271.1773
SLRCONSULTING.COM

DESCRIPTION | DATE | BY

SITE DETAILS
WAKE ROBIN INN REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

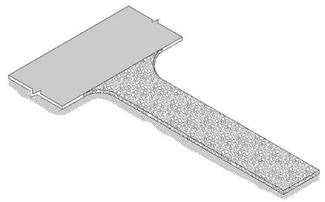
SM	DR	TR
DESIGNED	DRAWN	CHECKED

SCALE: AS NOTED
DATE: DECEMBER 9, 2024
PROJECT NO.: 22100.00001
SHEET NO.: 19 OF 23
SHEET NAME: SD-8

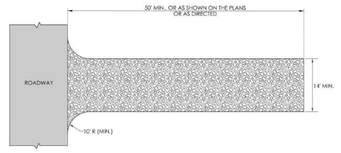
PLOTTED DATE: 9/27/2022

GENERAL NOTE:

1. THE LENGTH OF THE ANTI-TRACKING PAD SHALL BE INCREASED AS DIRECTED FOR SITES COMPOSED OF CLAY OR SILTS.

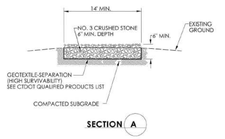


ANTI-TRACKING PAD



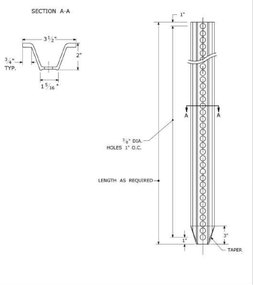
PLAN

ELEVATION

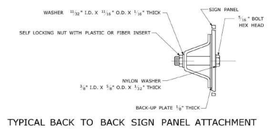


	NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: Digitally signed by Thomas Rinaldi Date: 2022.06.27 14:05:10 -04'00'	APPROVED BY: Digitally signed by Michael Laughlin Date: 2022.11.08 12:34:42 -05'00'	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	CTDOT STANDARD SHEET	STANDARD SHEET TITLE: ANTI-TRACKING PAD	STANDARD SHEET NO.: HW-211_01

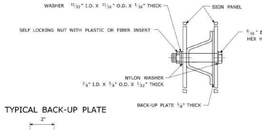
TYPICAL METAL SIGN POSTS



TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT

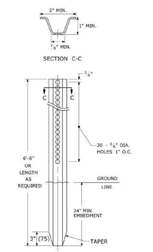


TYPICAL BACK-UP PLATE

BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240, (ALLOY TYPES 304 OR 316).

METAL DELINEATOR POST

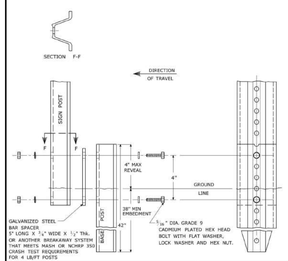
WT./FT. = 1.12 LBS./FT. MIN.



GENERAL NOTES:

1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 60 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
5. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
6. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
7. SIGN POSTS SHALL BE 4 LBS./FT.

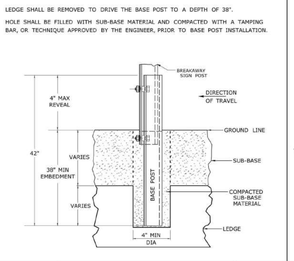
BREAKAWAY INSTALLATION
FOR 4 LBS./FT. POSTS



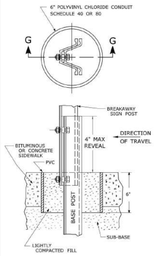
TYPICAL SIGN POST INSTALLATION IN LEDGE

LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38".

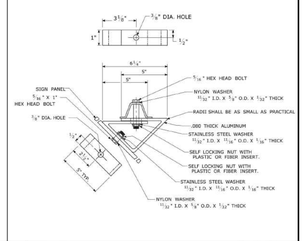
HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



TYPICAL SLEEVE
FOR PAVED AREAS



45° MOUNTING BRACKET
FOR INSTALLATION OF PARKING SIGNS



REV.	DATE	REVISION DESCRIPTION
2	4-2017	SIGN POST REVISIONS.
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES, CONTAINED IN THIS PLAN IS SUBJECT TO MINOR OR GREATER REVISIONS AND IS PROVIDED "AS IS" WITH NO WARRANTIES TO ANSWERS OR INTERPRETATIONS. ALL ESTIMATES OF WORK WHICH WILL BE REQUIRED.

PLEASE SEND 6/6/2017

	NOT TO SCALE	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	SUBMITTED BY: Digitally signed by Rick Elliott Date: 2017.06.19 07:57:52 -04'00'	APPROVED BY: Digitally signed by Gregory M. Dorsett, P.E. Date: 2017.06.19 09:27:26 -04'00'	CTDOT STANDARD SHEET	OFFICE OF ENGINEERING	METAL SIGN POSTS AND SIGN MOUNTING DETAILS	TR-1208_02

Filename: TR 1208 02 May 2017 Revised.dgn Model: TR 1208 02

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

DESCRIPTION	DATE	BY

SITE DETAILS

WAKE ROBIN INN
REDEVELOPMENT
104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

SM	DR	TR
DESIGNED	DRAWN	CHECKED

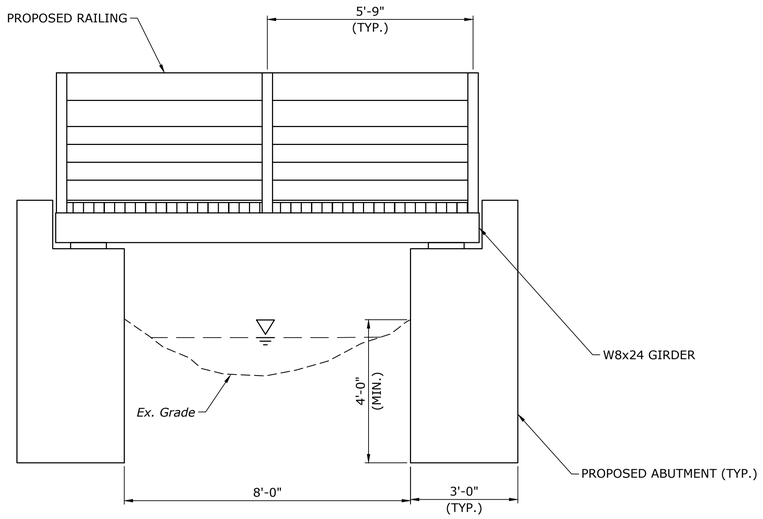
AS NOTED
SCALE

DECEMBER 9, 2024
DATE

22100.00001
PROJECT NO.

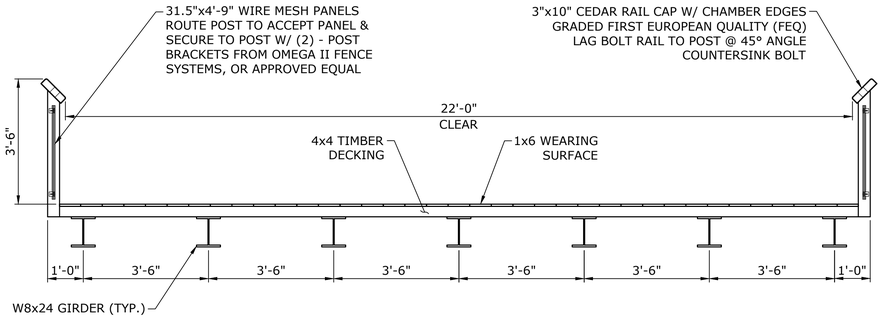
20 OF 23
SHEET NO.

SD-9
SHEET NAME



PROPOSED ELEVATION

SCALE: $\frac{1}{2}$"=1'-0"



TYPICAL BRIDGE SECTION (TIMBER)

SCALE: $\frac{1}{2}$"=1'-0"

SLR
99 REALTY DRIVE
CHESHIRE, CT 06410
203.271.1773
SLRCONSULTING.COM

DESCRIPTION	DATE	BY

STRUCTURAL DETAILS

WAKE ROBIN INN
REDEVELOPMENT

104 & 106 SHARON ROAD & 53 WELLS HILL ROAD
SALISBURY, CONNECTICUT

KP	DR	KP
DESIGNED	DRAWN	CHECKED

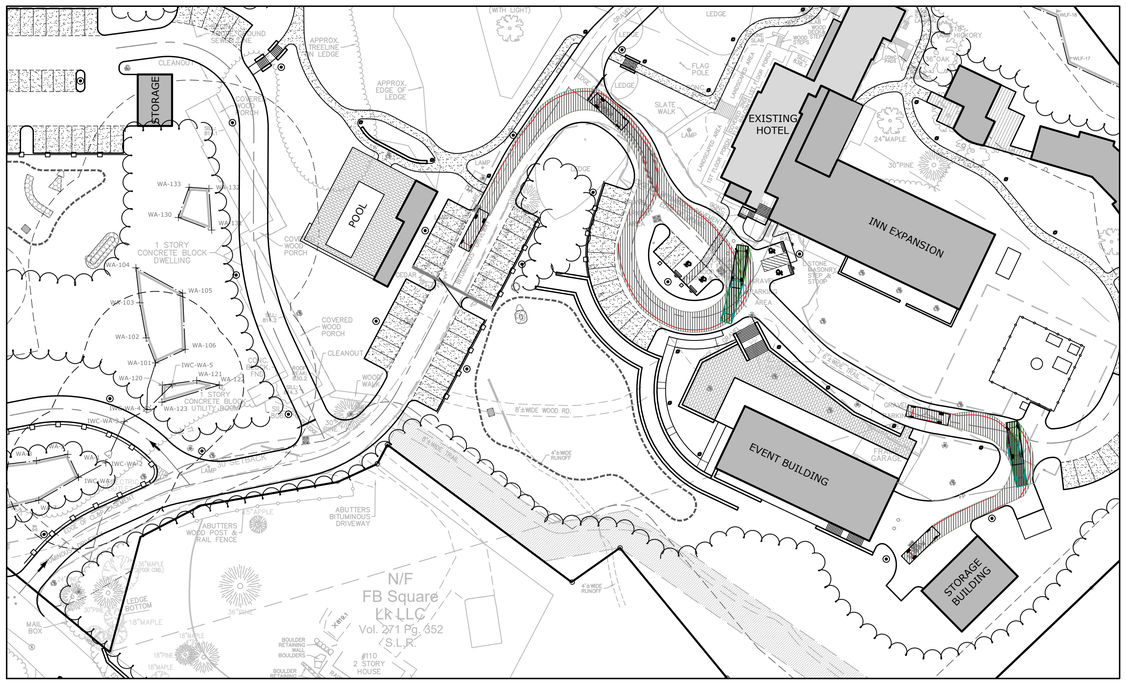
SCALE: 1/2"=1'-0"

DATE: SEPTEMBER 6, 2024

PROJECT NO.: 22100.00001

SHEET NO.: 21 OF 23

SHEET NAME: STR-1


STORAGE
EXISTING HOTEL
INN EXPANSION
POOL
EVENT BUILDING
STORAGE BUILDING
1 STORY CONCRETE BLOCK DWELLING
1 STORY CONCRETE BLOCK UTILITY BLDG
#110 2 STORY HOUSE
N/F FB Square LR LLC Vol. 271 Pg. 352 S.L.R.
ABUTTERS BITUMINOUS DRIVEWAY
ABUTTERS WOOD POST & RAIL FENCE
8'±WIDE WOOD RD.
4'±WIDE RUNOFF
COVERED WOOD PORCH
STONE MASONRY STEPS & STOOP
SLATE WALK
FLAG POLE
APPROX. TREELINE LEDGE
APPROX. EDGE OF LEDGE
LEDGE
CLEANOUT
MAIL BOX
BOULDER RETAINING WALL
WA-101
WA-102
WA-104
WA-105
WA-106
WA-120
WA-121
WA-123
WA-130
WA-133
IWC-WA-5
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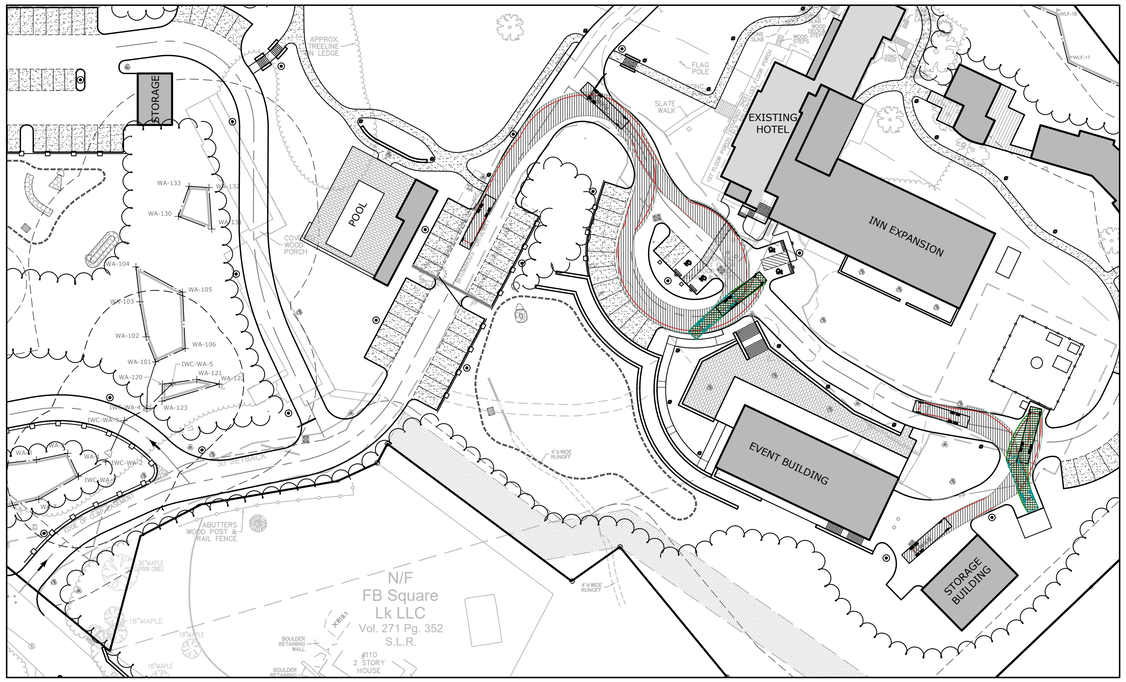
STORAGE
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#110
2 STORY
HOUSE
SLATE WALK
FLAG POLE
APPROX. TREELINE / LEDGE
COVERED WOOD PORCH
ABUTTERS WOOD POST & RAIL FENCE
BOULDER RETAINING WALL
4'-6 WIDE RUNOFF
WA-101
WA-102
WA-103
WA-104
WA-105
WA-106
WA-120
WA-121
WA-122
WA-123
WA-130
WA-133
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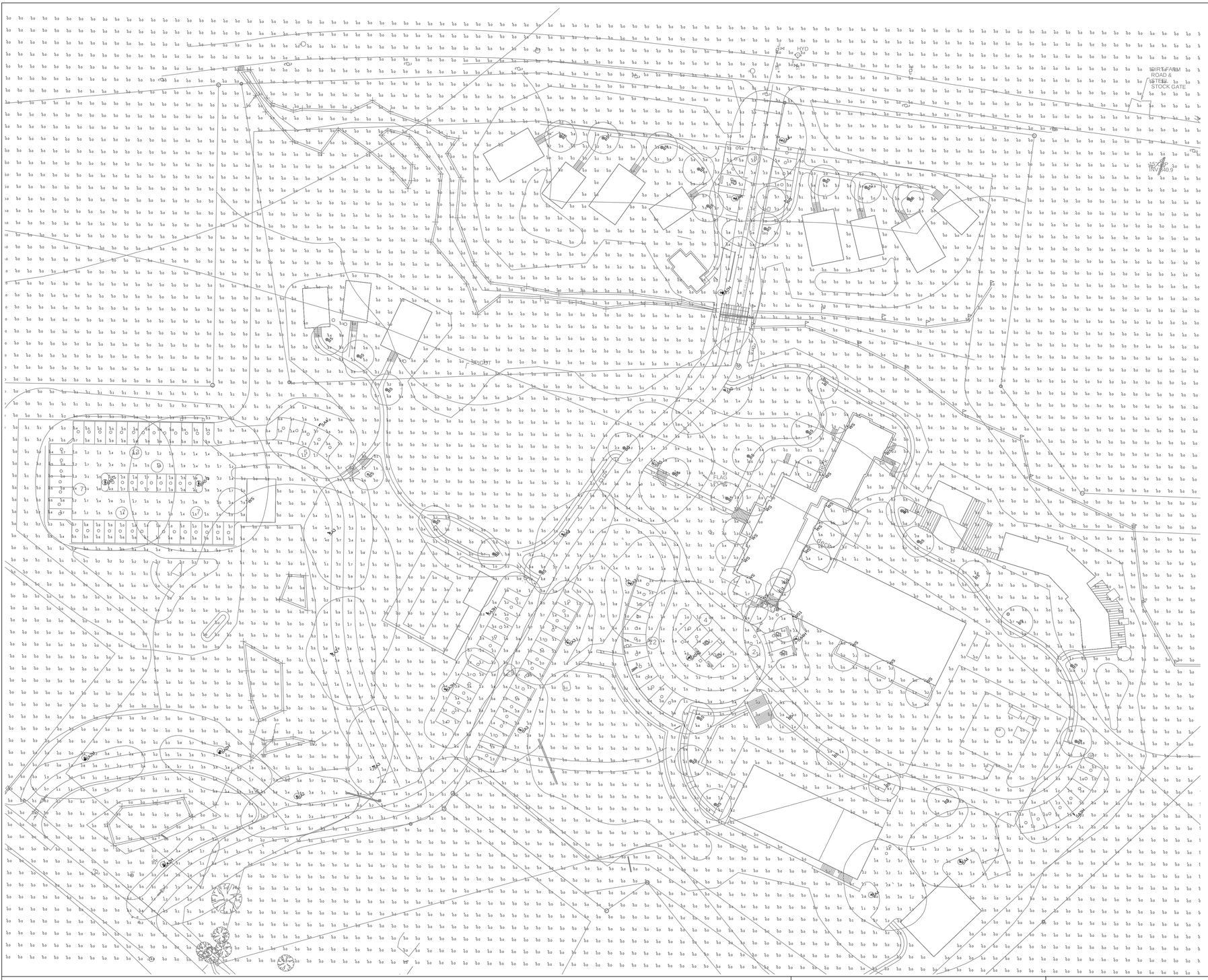

JOB NAME: WAKE ROBIN INN - 104-106 SHARON RD - SALISBURY, CT
APEX LIGHTING SOLUTIONS
WORKPLANE/CALC PLANE: AT FINISH GRADE
MOUNTING HEIGHT: SEE LUMINAIRE SCHEDULE
APPS: LEDPD
SALES: SP
SPECIFIER: SLR CONSULTING

Luminaire Schedule

Symbol	Qty	Label	Arrangement	Lum. Lumens	Lum. Watts	LLF	Description	[MANUFAC]	Filename
	35	BS	Single	1885	21.23	0.765	SRB8-F-X-2700K-UNV-FINISH-24in	Solera	srb8-f-x-449704.ies
	2	E-1	Single	840	8.16	0.850	N96C-4RTWMPW 2700K, 8ft MH	Nora Lighting	N96C-4RTWMPW 2700K.ies
	3	SA2	Single	7280	53.6	0.850	DSSC-PLED-II-48LED-350mA-27K / RNTS 144-11-P727-FINISH / XPKM-1-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-II-48LED-350mA-27K.ies
	4	SA2H	Single	5324	53.6	0.850	DSSC-PLED-II-48LED-350mA-27K-HS / RNTS 144-11-P727-FINISH / XPKM-1-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-II-48LED-350mA-27K-HS.ies
	6	SA3	Single	7407	53.6	0.850	DSSC-PLED-III-M-48LED-350mA-27K / RNTS 144-11-P727-FINISH / XPKM-1-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-III-M-48LED-350mA-27K.ies
	3	SA3H	Single	5386	53.6	0.850	DSSC-PLED-III-M-48LED-350mA-27K-HS / RNTS 144-11-P727-FINISH / XPKM-1-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-III-M-48LED-350mA-27K-HS.ies
	5	SA4	Single	7351	53.6	0.850	DSSC-PLED-IV-48LED-350mA-27K / RNTS 144-11-P727-FINISH / XPKM-1-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-IV-48LED-350mA-27K.ies
	1	SA4H	Single	5563	53.6	0.850	DSSC-PLED-IV-48LED-350mA-27K-HS / RNTS 144-11-P727-FINISH / XPKM-1-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-IV-48LED-350mA-27K-HS.ies
	1	SA5	Single	7355	53.6	0.850	DSSC-PLED-VSQ-W-48LED-350mA-27K / RNTS 144-11-P727-FINISH / XPKM-1-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-VSQ-W-48LED-350mA-27K.ies
	3	SA5-2	Back-Back	7355	53.6	0.850	DSSC-PLED-VSQ-W-48LED-350mA-27K / RNTS 144-11-P727-FINISH / XPKM-2-180-FINISH	U.S. ARCHITECTURAL LIGHTING	DSSC-PLED-VSQ-W-48LED-350mA-27K.ies
	2	SL	Single	91	3	0.765	LEDSTEP001D -2700K -FINISH, Mounted 1.5ft	DALS Lighting	LEDSTEP001D .IESNA2002.IES
	16	WS	Single	812	14	0.850	LBLED14-FINISH-3, Wall Mounted 8ft, 45° Tilt	Troy CSL Lighting Inc	LBLED-8L.IES
	1	WS1	Single	812	14	0.850	LBLED14-FINISH-3, Wall Mounted 8ft, 45° Tilt	Troy CSL Lighting Inc	LBLED-8L.IES

Calculation Summary

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min	Description
Site	Illuminance	Fc	0.30	18.9	0.0	N.A.	N.A.	10ft Grid
Event Barn Parking Lot	Illuminance	Fc	1.44	4.3	0.4	3.60	10.75	10ft Grid
Hotel Parking Loop	Illuminance	Fc	1.44	5.0	0.4	3.60	12.50	10ft Grid
Hotel Rear Deck	Illuminance	Fc	1.57	6.1	0.1	15.70	61.00	10ft Grid

GENERAL DISCLAIMER:

Calculations have been performed according to IES standards and good practice Some differences between measured values and calculated results may occur due to tolerances in calculation methods, testing procedures, component performance, measurement techniques and field conditions such as voltage and temperature variations. Input data used to generate the attached calculations such as room dimensions, reflectances, furniture and architectural elements significantly affect the lighting calculations. If the real environment conditions do not match the input data, differences will occur between measured values and calculated values.

* LLF Determined Using Current Published Lamp Data

NOTE TO REVIEWER:

Total Light Loss Factor (LLF) applied at time of design is determined by applying the Lamp Lumen Depreciation (LLD) from current lamp manufacturer's catalog, a Luminaire Dirt Depreciation Factor (LDD) based on IES recommended values and a Ballast Factor (BF) from current ballast specification sheets. Application of an incorrect Light Loss Factor (LLF) will result in forecasts of performance that will not accurately depict actual results.

For proper comparison of photometric layouts, it is essential that you insist all designers use correct Light Loss Factors.

APEX
LIGHTING SOLUTIONS
20-30 BEAVER ROAD, WETHERSFIELD, CT 06109
TELEPHONE 860.632.8766 / WWW.APEXLTG.COM

PROJECT TITLE:

WAKE ROBIN INN
104-106 SHARON RD
SALISBURY, CT

DRAWING TITLE:

SITE LIGHTING
PHOTOMETRIC CALCULATION

SCALE : 1"=40'-0"

DATE: 11/18/24

DRAWN BY: LED/PD

SHEET:

SL-1C

FILE NAME: 2024-11-18 SL-1C WAKE ROBIN INN - 104-106 SHARON RD - SALISBURY, CT.dwg